



DEPARTMENT OF CITY PLANNING

RECOMMENDATION REPORT

City Planning Commission

Date: June 8, 2023
Time: after 8:30 a.m.
Place: Los Angeles City Hall
Council Chambers, 3rd Floor
200 North Spring Street, Room 340
Los Angeles, CA 90012

This meeting may be available virtually, in a hybrid format. The meeting's telephone number and access code number will be provided no later than 72 hours before the meeting on the meeting agenda published at <https://planning.lacity.org/about/commissions-boards-hearings> and/or by contacting cpc@lacity.org

Case No.: CPC-2020-7285-GPA
CEQA No.: POLB EIR SCH No. 2009081079
Incidental Cases: None
Related Cases: None
Council No.: 15 - McOsker
Plan Area: Wilmington - Harbor City
Specific Plan: None
Certified NC: Wilmington
GPLU: Heavy Manufacturing
Zone: M3-1VL
Applicant: Port of Long Beach
Representative: Rob Katherman, The Ross Group

Public Hearing: March 15, 2023
Appeal Status: Not Applicable
Expiration Date: July 23, 2023
Multiple Approval: No

PROJECT LOCATION: 3200 East Anaheim Street, Los Angeles, CA 90744

PROPOSED PROJECT: The proposed project is the Port of Long Beach Anaheim Way Heavy Haul Route Project to realign the existing Anaheim Way to accommodate oversized truck turning movements along Anaheim Way from Pier B Street to Farragut Avenue. The realignment will enable oversized trucks (approximately 50 annually) to use this route with a police escort and an overweight truck route permit from the City of Los Angeles. The proposed project provides an alternative route for oversized trucks accessing the Port via Farragut Avenue - Anaheim Way - Anaheim Street replacing the 9th Street at-grade crossing route scheduled to be permanently closed under the Pier B On-Dock Rail Support Facility Program. The project will include widening of Anaheim Way from 45 feet to 72 feet, and Farragut Avenue from 44 feet to 72 feet, which would require reclassification from Local Industrial Streets to Collector Industrial Streets and inclusion into the Overweight Vehicle Special Permit Route. The project proposes new curbs/sidewalks, utilities, street lights, catch basin, and striping, etc. No changes to land use designations or zoning are proposed. No (0) protected trees are proposed to be removed.

REQUESTED ACTION: 1. Pursuant to California Environmental Quality Act ("CEQA") Guidelines, based on the whole of the administrative record, that the project was assessed in the previously certified Environmental Impact Report by the Port of Long Beach (SCH No. 2009081079), certified on January 22, 2018; and pursuant to CEQA Guidelines, Sections 15162 and 15164, no subsequent EIR, negative declaration, or addendum is required for approval of the project.

2. Pursuant to Los Angeles Municipal Code ("LAMC") Section 12.32, a General Plan Amendment to re-designate Anaheim Way from a Local Industrial Street to Collector Industrial Street.
3. Pursuant to LAMC Section 12.32, a General Plan Amendment to re-designate Farragut Avenue from a Local Industrial Street to Collector Industrial Street.

RECOMMENDED ACTIONS:

1. **Find**, based on the independent judgment of the decision-maker, after consideration of the whole of the administrative record, the project was assessed in the previously certified Environmental Impact Report by the Port of Long Beach (SCH No. 2009081079), certified on January 22, 2018; and pursuant to CEQA Guidelines, Sections 15162 and 15164, no subsequent EIR, negative declaration, or addendum is required for approval of the project.
2. **Approve and Recommend a General Plan Amendment** to the Wilmington – Harbor City Community Plan to re-designate Anaheim Way from an Industrial Local Street to Industrial Collector Street.
3. **Approve and Recommend a General Plan Amendment** to the Wilmington – Harbor City Community Plan to re-designate Farragut Avenue from an Industrial Local Street to Industrial Collector Street.

VINCENT P. BERTONI, AICP
Director of Planning

Theodore L. Irving

Theodore L. Irving, Principal City Planner

Michelle Singh

Michelle Singh, Senior City Planner

Connie Chauv

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ADVICE TO PUBLIC: *The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Written communications may be mailed to the *Commission Secretariat, Room 273, City Hall, 200 North Spring Street, Los Angeles, CA 90012* (Phone No. 213-978-1300). While all written communications are given to the Commission for consideration, the initial packets are sent to the week prior to the Commission's meeting date. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendaized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters, assistive listening devices, or other auxiliary aids and/or other services may be provided upon request. To ensure availability of services, please make your request not later than three working days (72 hours) prior to the meeting by calling the Commission Secretariat at (213) 978-1300.

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PROJECT ANALYSIS

PROJECT SUMMARY

The proposed project is the Port of Long Beach Anaheim Way Heavy Haul Route Project to realign the existing Anaheim Way to accommodate oversized truck turning movements along Anaheim Way from Pier B Street to Farragut Avenue. The roadway realignment will enable oversized trucks (approximately 50 annually) to use this route provided a police escort and an overweight truck route permit from the City of Los Angeles is obtained.

The project will provide an alternative route for oversized trucks accessing the Port via Farragut Avenue - Anaheim Way - Anaheim Street replacing the 9th Street at-grade crossing route scheduled to be permanently closed under the Pier B On-Dock Rail Support Facility Program.

The vacant property north of Anaheim Way will be used for the new alignment of the Anaheim Way oversized truck route, and the vacant property east of Farragut Avenue will be used to widen the street. The project will include widening of Anaheim Way from 45 feet to 72 feet, and Farragut Avenue from 44 feet to 72 feet, which would require reclassification from Industrial Local Streets to Industrial Collector Streets and inclusion into the Overweight Vehicle Special Permit Route. The project proposes new curbs/sidewalks, utilities, street lights, catch basin, and striping, etc. No changes to land use designations or zoning are proposed. No (0) protected trees are proposed to be removed.

The proposed project is part of the larger Pier B On-Dock Rail Support Facility Program that includes the reconfiguration, expansion, and enhancement of the capacity of the existing Pier B Rail Yard Facility. Port operations currently use an at-grade crossing at 9th Street for oversized trucks transporting large equipment, which accommodates approximately 50 trips annually, and is scheduled to be permanently closed under the Pier B On-Dock Rail Support Facility Program. The identified alternative route for oversized trucks accessing the Port is through Farragut Avenue – Anaheim Way – Anaheim Street, which require widening and improvements along Anaheim Way and Farragut Avenue to accommodate the oversized trucks, including the re-designation from Industrial Local Streets to Industrial Collector Streets to effectuate the change.

The larger Pier B On-Dock Rail Support Facility Program was analyzed in an Environmental Impact Report that was prepared by the Port of Long Beach (SCH No. 2009081079), certified on January 22, 2018. Based on the independent judgement of the decision-maker, after consideration of the whole of the administrative record, the proposed project was assessed in the previously certified EIR, and pursuant to CEQA Guidelines Sections 15162 and 15164: no subsequent EIR, negative declaration, or addendum is required for approval of the project.

BACKGROUND

Subject Property

The General Plan Amendment request is to re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets in the Wilmington – Harbor City Community Plan. The lots abutting Anaheim Way and Farragut Avenue are currently vacant and undeveloped. The abutting lots are within a 2000 foot buffer zone for a Border Zone Property (“BZP”) Site, are within a liquefaction zone, tsunami inundation zone, and coastal zone, and are approximately 4.5 kilometers of the Newport - Inglewood Fault Zone. The abutting lots are not within a designated hillside, airport hazard zone, fire hazard severity zone, flood zone, watercourse, special grading area, landslide area, or fault rupture area.

Zoning and Land Use Designation

The proposed project site is located within the Wilmington – Harbor City Community Plan. The lots abutting Anaheim Way and Farragut Avenue are currently vacant and undeveloped but are designated for Heavy Manufacturing land uses and are zoned M3-1VL. The M3 zone permits M2 uses, any industrial uses, nuisance type uses 500 feet from any other zone, except that no R zone uses are allowed. Height District No. 1VL permits 45 foot building height and Floor Area Ratio of 1.5:1.

Surrounding Uses

The proposed project site is located to the north of the Port of Long Beach. The immediately surrounding area is zoned M3-1VL and improved with industrial land uses or are otherwise vacant. The surrounding properties include automotive uses, open storage, and truck container yards across Farragut Avenue to the west; freight transport, waste management, truck yards, and open storage across I Street to the north and east; and Port-related uses and parking across Anaheim Street to the south. There are no residentially zoned properties within approximately 4,000 feet of the subject site.

Streets and Circulation

Anaheim Way is currently dedicated to a right-of-way width of approximately 64 feet and the roadway is approximately 45 feet. These dimensions most closely resemble the Standard Street Dimensions (per the Mobility Plan and Standard Plan S-470-1) of an Industrial Local Street, which has a designated right-of-way width of 64 feet and a designated roadway width of 44 feet.

Farragut Avenue is currently designated as a Local Street – Standard with a designated right-of-way width of 60 feet and roadway width of 36 feet. The dedicated right-of-way is approximately 60 feet and dedicated roadway is approximately 44 feet.

Anaheim Street is designated as a Boulevard II with a designated right-of-way width of 110 feet and roadway width of 80 feet. The dedicated right-of-way is approximately 100 feet and dedicated roadway is approximately 85 feet.

Public Transit

The site is within proximity to bus stops served by the Los Angeles County Metropolitan Transit Authority (“Metro”) 232 bus line.

Relevant Cases and Building Permits

Subject Site:

Case No. DIR-2020-7285-CDP: On December 7, 2020, an application was filed for a Coastal Development Permit for the Pier B On-Dock Rail Support Facility. The CDP was subsequently withdrawn on January 14, 2022, as the Public Project was processed by the Bureau of Engineering under CDP Permit No. 21-04. On February 9, 2022, the applicant filed for a General Plan Amendment under the same case number.

Surrounding Sites:

None.

REQUESTED ACTIONS**General Plan Amendment**

The subject application for a General Plan Amendment was submitted to the Department of City Planning on February 9, 2022, to re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets in the Wilmington – Harbor City Community Plan. The amendment was initiated by the Director of Planning on August 18, 2021.

CEQA

The Port of Long Beach prepared an Environmental Impact Report for the proposed project (SCH No. 2009081079), certified on January 22, 2018. Based on the independent judgement of the decision-maker, after consideration of the whole of the administrative record, the proposed project was assessed in the previously certified EIR, and pursuant to CEQA Guidelines Sections 15162 and 15164, no subsequent EIR, negative declaration, or addendum is required for approval of the project.

ISSUES**Public Hearing**

The public hearing was held on March 15, 2023 at approximately 10:00 a.m. Due to concerns over COVID-19, the Public Hearing was conducted in a virtual format. The public hearing was attended by the applicant's representative (Armen Ross, Rob Katherman, Mark Erickson) and approximately 5 members of the applicant team, as well as approximately eight (8) other members from the community. There were approximately six (6) speakers who provided comments at the hearing including representatives from the Wilmington Neighborhood Council (Valerie Contreras) and Council District 15 (Sergio Carillo).

Heavy Haul Route

Public comments at the hearing raised concerns regarding the change in the heavy haul route from the existing 9th Street crossing route (in the City of Long Beach) to the proposed Farragut Avenue - Anaheim Way - Anaheim Street route (in the City of Los Angeles). Specifically, the commenters requested additional protections and mitigations for residents due to the proximity of the new route to the Wilmington residential community. However, both existing and proposed routes lead to the Anaheim Street corridor, therefore there are no significant changes in the overall routes as heavy haul trucks will continue to lead to Anaheim Street. Furthermore, there are no residentially zoned properties within approximately 4,000 feet of the subject site.

CONCLUSION

The proposed project will re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets to accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region.

CONDITIONS OF APPROVAL

Development Conditions

1. **Site Plan.** The use and development of the subject property shall be in substantial conformance with the site plan labeled Exhibit "A". Prior to the issuance of building permits, detailed development plans including a site plan illustrating elevations, facades, and architectural treatment, and a landscape/irrigation plan shall be submitted for review and approval by the West/South/Coastal Project Planning Bureau of the Department of City Planning. The plans shall comply with provisions of the Municipal Code, the subject conditions, and the intent of the subject permit authorization.
2. **Street Designations, Dedications, and Improvements.**
 - a. Anaheim Way shall be designated an Industrial Collector Street. Improvements to the public right-of-way shall comply with Collector Industrial Standards in accordance with S-470-1 Standard Plans to the satisfaction of the Bureau of Engineering.
 - b. Farragut Avenue shall be designated an Industrial Collector Street. Improvements to the public right-of-way shall comply with Collector Industrial Standards in accordance with S-470-1 Standard Plans to the satisfaction of the Bureau of Engineering.
3. **Fire.** Submit plot plans for Fire Department approval and review prior to issuance of building permits.
4. **Landscape Plan.** Revised landscape plans shall be submitted to show the size and location of all plants. The landscape plan shall indicate landscape points for the Project as required by LAMC 12.40 and Landscape Ordinance Guidelines "O". All open areas not used for buildings, driveways, parking areas, recreational facilities or walks shall be landscaped, including an automatic irrigation system, and maintained in accordance with a final landscape plan prepared by a licensed landscape architect or licensed architect, and submitted for approval to the Department of City Planning. The final landscape plan shall be in substantial conformance with the submitted Landscape Plan, Exhibit "A," and shall incorporate any modifications required as a result of this grant.
5. **Street Trees.**
 - a. Project shall preserve all healthy mature street trees whenever possible. All feasible alternatives in project design should be considered and implemented to retain healthy mature street trees. A permit is required for the removal of any street tree and shall be replaced 2:1 as approved by the Board of Public Works and Urban Forestry Division.
 - b. Plant street trees at all feasible planting locations within dedicated streets as directed and required by the Bureau of Street Services, Urban Forestry Division. All street tree plantings shall be installed to current tree planting standards when the City has previously been paid for tree plantings. The sub divider or contractor shall notify the Urban Forestry Division at: (213) 847-3077 upon completion of construction for tree planting direction and instructions.

Note: Removal of street trees requires approval from the Board of Public Works. All projects must have environmental (CEQA) documents that appropriately address any removal and replacement of street trees. Contact Urban Forestry Division at: (213) 847-3077 for tree removal permit information.

6. New trees planted within the public right-of-way shall be spaced not more than an average of 30 feet on center, unless otherwise permitted by the Urban Forestry Division, Bureau of Public Works.

Environmental Conditions

7. **Mitigation Monitoring and Reporting Program.** The Project shall comply with the Mitigation Measures set forth in the Mitigation Monitoring and Reporting Program, certified on January 22, 2018 and attached as Exhibit D-1, for which the Port of Long Beach or any City department is identified as a Monitoring Party, as may be amended by the Port of Long Beach.
8. **Construction Monitor.** During the construction phase and prior to the issuance of building permits, the Applicant shall retain an independent Construction Monitor (either via the City or through a third-party consultant), approved by the Department of City Planning, who shall be responsible for monitoring implementation of PDFs and MMs for which the City or any City department is identified as a Monitoring Party during construction activities consistent with the monitoring phase and frequency set forth in the MMRP (attached as Exhibit D-1). The Construction Monitor shall also prepare documentation of the Applicant's compliance with the PDFs and MMs during construction every 90 days in a form satisfactory to the Department of City Planning. The documentation must be signed by the Applicant and Construction Monitor and be included as part of the Applicant's Compliance Report. The Construction Monitor shall be obligated to immediately report to the Enforcement Agency any non-compliance with the MMs and PDFs within two businesses days if the Applicant does not correct the non-compliance within a reasonable time of notification to the Applicant by the monitor or if the non-compliance is repeated. Such non-compliance shall be appropriately addressed by the Enforcement Agency.

Administrative Conditions of Approval

9. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review of approval, plans, etc., as may be required by the subject conditions, shall be provided to the Department of City Planning prior to clearance of any building permits, for placement in the subject file.
10. **Code Compliance.** Use, area, height, and yard regulations of the zone classification of the subject property shall be complied with, except where granted conditions differ herein.
11. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assign. The agreement must be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the file.
12. **Definition.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public offices, legislation or their successors, designees or amendment to any legislation.

13. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning.
14. **Building Plans.** A copy of the first page of this grant and all Conditions and/or any subsequent appeal of this grant and its resultant Conditions and/or letters of clarification shall be printed on the building plans submitted to the Development Services Center and the Department of Building and Safety for purposes of having a building permit issued.
15. **Corrective Conditions.** The authorized use shall be conducted at all times with due regard for the character of the surrounding district, and the right is reserved to the City Planning Commission, or the Director pursuant to Section 12.27.1 of the Municipal Code, to impose additional corrective conditions, if, in the Commission's or Director's opinion, such conditions are proven necessary for the protection of persons in the neighborhood or occupants of adjacent property.
16. **Indemnification and Reimbursement of Litigation Costs.**

Applicant shall do all of the following:

- (i) Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- (ii) Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
- (iii) Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- (iv) Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- (v) If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions includes actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

FINDINGS

General Plan/Charter Findings

1. **Charter Finding – City Charter Finding 555.** The General Plan may be amended in its entirety, by subject elements or parts of subject elements, or by geographic areas, provided that the part or area involved has significant social, economic, or physical identity.

The proposed Project Site is located within the Wilmington – Harbor City Community Plan area, north of the Port of Long Beach. The immediately surrounding area is zoned M3-1VL and improved with industrial land uses or are otherwise vacant. The surrounding properties include automotive uses, open storage, and truck container yards across Farragut Avenue to the west; freight transport, waste management, truck yards, and open storage across I Street to the north and east; and Port-related uses and parking across Anaheim Street to the south. Due to the site's proximity to the Port of Long Beach, the site is critical in the movement of goods from the Port and therefore has significant economic identity.

2. **Charter Finding – City Charter Finding 556.** When approving any matter listed in Section 558, the City Planning Commission and the Council shall make findings showing that the action is in substantial conformance with the purposes, intent and provisions of the General Plan. If the Council does not adopt the City Planning Commission's findings and recommendations, the Council shall make its own findings.

The proposed Project Site is located within the Wilmington – Harbor City Community Plan area, which is one of 35 community plans that the Land Use Element of the General Plan is comprised of. The Community Plan does not identify a street classification or designation for Anaheim Way, however it designates Farragut Avenue as a Local Street – Standard.

As recommended, the General Plan Amendment would re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets. The amendment would accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region. As further discussed in Finding Nos. 4 and 5 through 7, the amendment of the street designation would be consistent with the purpose, intent, and provisions of the General Plan.

3. **Charter Finding – City Charter Finding 558.** The proposed Amendment to the Wilmington – Harbor City Community Plan will be in conformance with public necessity, convenience, general welfare and good zoning practice.

The recommended amendment to the Wilmington – Harbor City Community Plan would re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets to accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region.

Public Necessity, Convenience, and General Welfare

The Community Plan does not identify a street classification or designation for Anaheim Way, however it designates Farragut Avenue as a Local Street – Standard. The General Plan Amendment request is to re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets in the Wilmington – Harbor City Community Plan. The lots abutting Anaheim Way and Farragut Avenue are currently vacant and undeveloped.

The proposed project is the Port of Long Beach Anaheim Way Heavy Haul Route Project to realign the existing Anaheim Way to accommodate oversized truck turning movements along Anaheim Way from Pier B Street to Farragut Avenue. The changes will enable oversized trucks (approximately 50 annually) to use this route with police escort and the overweight truck route permit from the City of Los Angeles.

The project will provide an alternative route for oversized trucks accessing the Port via Farragut Avenue - Anaheim Way - Anaheim Street to replace the 9th Street at-grade crossing route scheduled to be permanently closed under the Pier B On-Dock Rail Support Facility Program.

The vacant property north of Anaheim Way will be used for the new alignment of the Anaheim Way oversized truck route, and the vacant property east of Farragut Avenue will be used to widen the street; both properties are owned by the Port of Long Beach. The proposed alignment will include widening of Anaheim Way from 45 feet to 72 feet, and Farragut Avenue from 44 feet to 72 feet, which would require reclassification from Industrial Local Streets to Industrial Collector Streets and inclusion into the Overweight Vehicle Special Permit Route. The project proposes new curbs/sidewalks, utilities, street lights, catch basin, and striping, etc. No changes to land use designations or zoning are proposed. No (0) protected trees are proposed to be removed.

The proposed project is part of the larger Pier B On-Dock Rail Support Facility Program that includes the reconfiguration, expansion, and enhancement of the capacity of the existing Pier B Rail Yard Facility. Port operations currently use an at-grade crossing at 9th Street for oversized trucks transporting large equipment, which accommodates approximately 50 trips annually, and is scheduled to permanently be closed under the Pier B On-Dock Rail Support Facility Program. The identified alternative route for oversized trucks accessing the Port is through Farragut Avenue – Anaheim Way – Anaheim Street, which require widening and improvements along Anaheim Way and Farragut Avenue to accommodate the oversized trucks, including the re-designation from Industrial Local Streets to Industrial Collector Streets to effectuate the change.

The proposed project will re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets to accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region. Due to the site's proximity to the Port of Long Beach, the site is critical in the movement of goods from the Port and serves public necessity and general welfare.

Good Zoning Practice

Anaheim Way is currently dedicated to a right-of-way width of approximately 64 feet and the roadway is approximately 45 feet. These dimensions most closely resemble the Standard Street Dimensions (per the Mobility Plan and Standard Plan S-470-1) of an Industrial Local Street, which has a designated right-of-way width of 64 feet and a designated roadway width of 44 feet.

Farragut Avenue is currently designated as a Local Street – Standard with a designated right-of-way width of 60 feet and roadway width of 36 feet. The dedicated right-of-way is approximately 60 feet and dedicated roadway is approximately 44 feet.

As recommended, the General Plan Amendment would re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets. The recommended amendment would be in substantial conformance with the purpose, intent, and

provisions of the General Plan as it is reflected within the Wilmington – Harbor City Community Plan, as further discussed in Finding Nos. 4 through 7.

The site is located to the north of the Port of Long Beach. The immediately surrounding area is zoned M3-1VL and improved with industrial land uses or are otherwise vacant. The surrounding properties include automotive uses, open storage, and truck container yards across Farragut Avenue to the west; freight transport, waste management, truck yards, and open storage across I Street to the north and east; and Port-related uses and parking across Anaheim Street to the south. There are no residentially zoned properties within 4,000 feet of the subject site.

The proposed project will re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets to accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region. Due to the site's proximity to the Port of Long Beach, the site is critical in the movement of goods from the Port. Furthermore, the vacant properties to be used for the new alignments are under the ownership of the Port of Long Beach. No privately-owned properties nor residentially zoned properties will be directly affected by the re-designation.

4. **General Plan Text / General Plan Designation.** The Project Site is located within the Wilmington – Harbor City Community Plan, which was adopted by the City Council on July 14, 1999, and amended on September 7, 2016 for the Mobility Plan 2035 Update.

Anaheim Way is currently dedicated to a right-of-way width of approximately 64 feet and the roadway is approximately 45 feet. These dimensions most closely resemble the Standard Street Dimensions (per the Mobility Plan and Standard Plan S-470-1) of an Industrial Local Street, which has a designated right-of-way width of 64 feet and a designated roadway width of 44 feet.

Farragut Avenue is currently designated as a Local Street – Standard with a designated right-of-way width of 60 feet and roadway width of 36 feet. The dedicated right-of-way is approximately 60 feet and dedicated roadway is approximately 44 feet.

As recommended, the General Plan Amendment would re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets. The recommended amendment would be in substantial conformance with the purpose, intent, and provisions of the General Plan as it is reflected within the Wilmington – Harbor City Community Plan, as further discussed in Finding Nos. 5 through 7.

The Wilmington - Harbor City Community Plan text includes the following relevant objectives, policies, and programs:

- | | |
|----------------|--|
| Goal 15 | To the extent feasible and consistent with the Mobility Plan 2035's and Community Plans' policies promoting multi-modal transportation and safety, a system of freeways, and streets that provides a circulation system which supports existing, approved, and planned land uses while maintaining a desired level of service at intersections. |
| Objective 15-1 | To the extent feasible and consistent with the Mobility Plan 2035's and the Community Plans' policies promoting multi-modal transportation and safety, comply with Citywide performance standards for acceptable levels of service (LOS) and insure that necessary road access and street improvements are provided to accommodate traffic generated by new development. |

- Policy 15-1.2 Street dedications shall be developed in accordance with standards and criteria contained in the Mobility Plan, an element of the General Plan and the City's Standard Street Dimensions, except where environmental issues and planning practices warrant alternate standards consistent with capacity requirements.

The site is located to the north of the Port of Long Beach. Due to the site's proximity to the Port of Long Beach, the site is critical in the movement of goods from the Port. The proposed project will provide an alternative route for oversized trucks accessing the Port via Farragut Avenue - Anaheim Way - Anaheim Street replacing the 9th Street at-grade crossing route scheduled to be permanently closed under the Pier B On-Dock Rail Support Facility Program. The project will be required to dedicate and improve the public right-of-way in accordance with S-470-1 Standard Plans to the satisfaction of the Bureau of Engineering. Furthermore, the vacant properties to be used for the new alignments are under the ownership of the Port of Long Beach. No privately-owned properties nor residentially zoned properties will be directly affected by the re-designation.

As recommended, the General Plan Amendment to re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets would be consistent with the above referenced objectives, policies, and programs of the Wilmington – Harbor City Community Plan.

5. **Framework Element.** The Framework Element for the General Plan (Framework Element) was adopted by the City of Los Angeles in December 1996 and re-adopted in August 2001. The Framework Element provides guidance regarding policy issues for the entire City of Los Angeles, including the project site.

The Framework Element identifies the following Transportation issue:

- Issue 5: Economic growth is essential to the long- term future of the City. To support all facets of the City's economy, the movement of goods must be efficient and access to major intermodal facilities such as ports, airports, and major multimodal facilities must be adequate. It is equally important that ground access to key transportation facilities is readily available.

In addition, the Framework Element also sets forth a Citywide comprehensive long-range growth strategy and defines Citywide policies regarding such issues as land use, housing, urban form, neighborhood design, open space, economic development, transportation, infrastructure, and public services. The Framework Element includes the following provisions, objectives and policies relevant to the instant request:

- Goal 3J: Industrial growth that provides job opportunities for the City's residents and maintains the City's fiscal viability.
- Objective 3.14: Provide land and supporting services for the retention of existing and attraction of new industries.
- Policy 3.14.1: Accommodate the development of industrial uses in areas designated as "Industrial-Light," "Industrial-Heavy," and "Industrial-Transit" in accordance with Tables 3-1 and 3-9. The range and intensities of uses permitted in any area shall be determined by the community plans.

- Policy 3.14.6: Consider the potential re-designation of marginal industrial lands for alternative uses by amending the community plans based on the following criteria:
- a. Where it can be demonstrated that the existing parcelization precludes effective use for industrial or supporting functions and where there is no available method to assemble parcels into a unified site that will support viable industrial development;
 - b. Where the size and/or the configuration of assembled parcels are insufficient to accommodate viable industrial development;
 - c. Where the size, use, and/or configuration of the industrial parcels adversely impact adjacent residential neighborhoods;
 - d. Where available infrastructure is inadequate and improvements are economically infeasible to support the needs of industrial uses;
 - e. Where the conversion of industrial lands to an alternative use will not create a fragmented pattern of development and reduce the integrity and viability of existing industrial areas;
 - f. Where the conversion of industrial lands to an alternative use will not result in an adverse impact on adjacent residential neighborhoods, commercial districts, or other land uses;
 - g. Where it can be demonstrated that the reduction of industrial lands will not adversely impact the City's ability to accommodate sufficient industrial uses to provide jobs for the City's residents or incur adverse fiscal impacts; and/or
 - h. Where existing industrial uses constitute a hazard to adjacent residential or natural areas.
- Policy 3.14.8: Encourage the development in areas designated as "Industrial-Heavy" of critical public facilities that are necessary to support the needs of residents and businesses but normally are incompatible with residential neighborhoods and commercial districts, such as corporate yards.

As recommended, the General Plan Amendment would re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets to accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region. The vacant property north of Anaheim Way will be used for the new alignment of the Anaheim Way oversized truck route, and the vacant property east of Farragut Avenue will be used to widen the street. The vacant properties to be used for the new alignments are under the ownership of the Port of Long Beach, are irregular in shape, and are not viable for industrial development, and are therefore appropriate to be used in the re-alignment and re-designation of Anaheim Way and Farragut Street.

As recommended, the re-designation will allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region, and would be consistent with the above referenced goals, objectives, and policies, of the Framework Element.

6. **Mobility Element.** The General Plan Amendment request is to re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets in the Wilmington – Harbor City Community Plan.

Anaheim Way is currently dedicated to a right-of-way width of approximately 64 feet and the roadway is approximately 45 feet. These dimensions most closely resemble the Standard Street Dimensions (per the Mobility Plan and Standard Plan S-470-1) of an Industrial Local Street, which has a designated right-of-way width of 64 feet and a designated roadway width of 44 feet.

Farragut Avenue is currently designated as a Local Street – Standard with a designated right-of-way width of 60 feet and roadway width of 36 feet. The dedicated right-of-way is approximately 60 feet and dedicated roadway is approximately 44 feet.

The project will be required to dedicate and improve the public right-of-way in accordance with S-470-1 Standard Plans to the satisfaction of the Bureau of Engineering. In addition to providing dedications to meet the established Street Standards, the project is also consistent with the following policies of the Mobility Element:

- Policy 1.8: Goods Movement Safety: Ensure that the goods movement sector is integrated with the rest of the transportation system in such a way that does not endanger the health and safety of residents and other roadway users.
- Policy 2.8: Goods Movement: Implement projects that would provide regionally significant transportation improvements for goods movement.
- Policy 2.9: Multiple Networks: Consider the role of each enhanced network when designing a street that includes multiple modes.
- Policy 2.14: Street Design: Designate a street's functional classification based upon its current dimensions, land use context, and role.
- Policy 3.1: Access for All: Recognize all modes of travel, including pedestrian, bicycle, transit, and vehicular modes - including goods movement - as integral components of the City's transportation system.
- Policy 4.12: Goods Movement: Increase public awareness about the importance and economic value of goods movement in the Los Angeles region.
- Policy 5.1: Sustainable Transportation: Encourage the development of a sustainable transportation system that promotes environmental and public health.

The proposed project will re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets to accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region. The project proposes new curbs/sidewalks, utilities, street lights, catch basin, and striping, etc.

Entitlement Findings**7. Land Use Legislative Findings.**

- a. **Pursuant to Section 12.32-C of the Municipal Code, and based on these findings, the recommended action is deemed consistent with public necessity, convenience, general welfare and good zoning practice.**

As provided under Finding No. 3, the proposed amendment will be in conformance with public necessity, convenience, general welfare and good zoning practice. The proposed project will re-designate Anaheim Way and Farragut Avenue from Industrial Local Streets to Industrial Collector Streets to accommodate oversized truck turning movements and allow the continued movement of goods from the Port of Long Beach to serve the Los Angeles region. Due to the site's proximity to the Port of Long Beach, the site is critical in the movement of goods from the Port and serves public necessity and general welfare. Furthermore, the vacant properties to be used for the new alignments are under the ownership of the Port of Long Beach. No privately-owned properties nor residentially zoned properties will be directly affected by the re-designation.

Environmental Findings

10. **Environmental Impact Report.** The Port of Long Beach prepared an Environmental Impact Report for the proposed project (SCH No. 2009081079), certified on January 22, 2018. Based on the independent judgement of the decision-maker, after consideration of the whole of the administrative record, the project was assessed in the previously certified EIR, and pursuant to CEQA Guidelines Sections 15162 and 15164, no subsequent EIR, negative declaration, or addendum is required for approval of the project. The records upon which this decision is based are provided in Exhibit D and available with the Project Planning Division of the Planning Department in Room 721, 200 North Spring Street.
11. **Flood Insurance.** The National Flood Insurance Program rate maps, which are a part of the Flood Hazard Management Specific Plan adopted by the City Council by Ordinance No. 172,081, have been reviewed and it has been determined that this project is located outside the flood zone.

PUBLIC HEARING AND COMMUNICATIONS

PUBLIC HEARING

The public hearing was held on March 15, 2023 at approximately 10:00 a.m. Due to concerns over COVID-19, the Public Hearing was conducted in a virtual format. The hearing was conducted by the Hearing Officer, Connie Chauv, on behalf of the City Planning Commission in taking testimony for Case No. CPC-2020-7285-GPA. All interested parties were invited to attend the public hearing at which they could listen, ask questions, or present testimony regarding the project. The purpose of the hearing was to obtain testimony from affected and/or interested parties regarding this application. Interested parties are also invited to submit written comments regarding the request prior to the hearing. The environmental analysis was among the matters to be considered at the hearing. The hearing notice was mailed on February 13 and February 17, 2023, published in the newspaper on February 17, 2023, and was posted on-site on February 28, 2023, in accordance with LAMC noticing requirements. The courtesy notice was mailed on May 25, 2023, and was posted on-site on May 28, 2023, in accordance with LAMC noticing requirements.

The public hearing was attended by the applicant's representative (Armen Ross, Rob Katherman, Mark Erickson) and approximately 5 members of the applicant team, as well as approximately eight (8) other members from the community. There were approximately six (6) speakers who provided comments at the hearing including representatives from the Wilmington Neighborhood Council (Valerie Contreras) and Council District 15 (Sergio Carillo).

Applicant Presentation. The applicant's representative described the site location, project description, relationship to the larger project, and requested entitlements. Specifically, the applicant noted the following:

- Port of Long Beach approved their CDP 5 years ago. City of Los Angeles approved their CDP in 2022 including the widening. The site is in the dual permit zone.
- Technical Memorandum dated June 23, 2022 found that the Anaheim Way widening was covered in the prior EIR, that the project would not result in any new significant environmental effects, would not substantially increase the severity of previously identified significant effects, and that there is no new information of substantial importance showing any new significant effects, any substantially more severe effects, mitigation measures or alternatives previously found to be infeasible would in fact be feasible, or mitigation measures or alternatives that are considerably different would substantially reduce significant effects. The project was within the scope of the EIR. No new environmental effects would occur. All feasible mitigation measures from the previous EIR have been incorporated into the project
- The overall Pier B On-Dock Rail Support Facility Project includes a resupply station for up to 30 locomotives, a 93,000 square-foot support yard, and 5 new arrival and departure tracks. Most of the project is in Long Beach but approximately 20-25 percent is in the City of Los Angeles.
- Overall program benefits include higher ACTA revenue due to rail growth, improved competitiveness with other harbors, mode shift from truck to rail, lower emissions, and reduced shipping cost.
- The subject project is the realignment of Anaheim Way and Farragut Street. This route is vitally important to get the Port of Long Beach capability to export. The closest alternative with capability to handle this cargo is in the Gulf of Houston in Texas.
- There would be approximately 50 over-sized shipments per year, all will be escorted by law enforcement with proper permits from City of Los Angeles and other permitting entities.

- The proposed new route will allow the Port to continue to handle these shipments, and includes economic and regional benefits.
- The oversized cargo route is equipped to move vaporizers, compressors, and similar cargo needed for electrical infrastructure improvements.
- Port of Long Beach is a strategic seaport for military movements, which may be reduced without an alternative route to the 9th Street crossing which would be permanently closed.
- Trailers are approximately 220 feet in length, with up to 60 tires, and need wide turning radius.
- Project will provide an alternative route, as the existing 9th Street crossing will be closed. From Pico, route will travel north/northwest along Pier B Street, connecting to Anaheim Way, continue northwest along Anaheim Way, turn west to Farragut Avenue, and then back south onto Anaheim Street, so trucks can go either west or east on Anaheim Street.
- The oversized truck turning template shows that it will encroach onto the current curb and sidewalk by approximately 25 feet, so the turning radius has to be expanded. Trucks need to swing out or north in order to make that turn coming south. Farragut would also be impacted by turning south, and would also need to be widened.
- Both sides of the street are vacant land owned by the Port of Long Beach. They are zoned for manufacturing. There are no residences or buildings, so there is no impact on general public as far as continuing to use the street for general public purpose.
- The street will be striped so that normal traffic pattern will be kept. Additional striping will show where big trucks would make their turn going south down Farragut Avenue.
- The request is a General Plan Amendment to re-designate Anaheim Way and Farragut Avenue from Local to Collector. Anaheim would be widened from 45 feet to 72 feet. Farragut would be widened from 44 feet to 72 feet.
- Other improvements would include:
 - New curbs and sidewalks with drought-tolerant landscaping along the roadway
 - Existing utilities, street lights, and a catch basin will be reconstructed
 - Traffic signal pole, street light, and infrastructure to be replaced
 - Additional street lighting
 - Striping to match the existing roadway widths and lane assignments

Wilmington Neighborhood Council:

- Applicant first came to the Neighborhood Council in 2017.
- Both Port of Los Angeles and Port of Long Beach will be using the rail project and road.
- Has heard about how the project will benefit the Port, but did not hear about how it will benefit the community.
- Port of Long Beach has cut off access for Wilmington commuters to easily access the POLB berths, and wants to widen the roadways to flow trucks from the Port into Wilmington.
- Truck routes were used previously on 9th Street going towards Long Beach, but now majority will be going towards Wilmington into a heavily congested area.
- Anaheim is now down to one lane due to bike lane, and poses a danger and risk.
- Heavy truck escorts poses dangers with accidents, cargo falling onto cars, causing traffic.
- Wilmington is overburdened by both Ports.
- The address is 3200 Anaheim but the realignment is along Anaheim Way
- Very concerned about the entire project.
- Port of Long Beach has rail along the neighborhood but did not provide sound barriers, walls, or trees. The whole area should be canvassed by trees because they are heavily impacting the environment.
- Transportation will be affected by the project.
- Requests to deny the General Plan Amendment on behalf of the community.
- The project was already counting on this as they are closing 9th Street.

Public Comments in Opposition:

- Project should require more mitigation. The project is giving a lot of mitigation only to Long Beach, but Wilmington is not receiving mitigation.
- Should look to LAWA for mitigation measures, for examples they looked at sound barriers next to school.
- Should look at arts.
- EIR only studied 4 sensitive areas within 1 mile, but should look at Alameda.
- Bike lanes and reduced lanes were not studied in the EIR.
- Applicant mentions only 20-25 percent in Los Angeles, but it is more than 50 percent.
- There will be more pollution so it will impact the residential.
- Wilmington is a community of color that is overburdened with heavy trucks and pollution. Adding one more thing is very bad.
- More trucks coming in will create a bottleneck.
- Wilmington has the highest pollution in Southern California.
- Want to see benefits for the community like a hospital or for preparation of earthquakes.

General Questions:

- What impacts will affect the areas west of Farragut to the Dominguez Channel and approximately to I Street up to the rail right-of-way? Can they assume there will be no impedance of traffic on Anaheim Street?
- Is there any involvement of condominium properties owned by Port of Long Beach and Port of Los Angeles in middle of I Street area?

Council District 15:

- Council Office requests file be kept open for 30 days for additional public comment.
- Council Office is in active discussions with the Port of Long Beach on benefits, and would like time to continue discussions. Plans to submit more formal comment following discussion with POLB.
- Wilmington has bore the brunt of Port of Long Beach operations. Pier B is a significant project with regional impacts.

Applicant's Response to Public Comments and Staff Questions:

- Applicant submitted comprehensive response addressing Neighborhood Council comment letter, including sound barriers, landscaping, truck flow, pollution, noise, etc.
- The project does not qualify for the 1 percent arts fee.
- Applicant has been sending notices to Neighborhood Council about community grants program.
- The project does not bring more trucks into the area; it is only re-routing the current program for approximately 50 trucks per year to get to Anaheim Street. It does not increase the number of trucks using the route.
- Their vision for the future is for both Ports to work together to transition to a cleaner Port. It fits within many goals to reduce impacts to transportation, generate jobs, strengthen and improve efficiency.
- The Port is a key driver in the regional economy and national economy. Major construction projects like the LA Metro, SoCalEdison clean grid, etc are coming through here.
- POLB presented a zero emission program at the State of the Port to look at energy resiliency, move to zero emissions by 2035, and call cargo handling equipment to zero emissions by 2030. They are looking to shift modes to shift from truck to train traffic, which will be good for the regional highway network and for climate change.

- POLB has a \$60 million community grant program that includes Wilmington. They are talking to the Chamber of Commerce about strengthening bonds between communities, and want to be a positive impact on the community.
- The project is a small piece of the total Pier B On-Dock Rail Support Facility Program. It will allow the Port to continue with the Heavy Haul Route Project. It was evaluated in the EIR. The re-designation is required because they need to widen the street to allow turns. They analyzed other ways for trucks to make turns but were unsuccessful. This was the only option to widen the street.
- The Certified EIR was approved in 2018 and looked at all impacts under CEQA. Each item was addressed including air quality, greenhouse gas, and noise. The findings resulted in mitigation measures including dust control, Tier 4 engines for construction vehicles, etc. They expect to see growth in freight which was analyzed in the EIR, but expect zero emissions for the future as the goal and intent.
- No grade crossings will be affected by the project.
- There are no residences in the immediate vicinity of the railyard. The nearest residential is approximately one-half mile away across from Anaheim Street.
- The properties along Anaheim Way and Farragut Avenue are Port-owned property. They are currently vacant. They may have been used for construction laydown area or staging for the Port in the past 20 years, but they are unimproved dirt lots.
- Applicant has done attended public stakeholder outreach on the broader Pier B program with Wilmington Chamber of Commerce and Neighborhood Council. Applicant has already reached out to Neighborhood Council to give updates on the project, and are working on scheduling another briefing. Applicant will work with Council District.

WRITTEN CORRESPONDENCE

The Wilmington Neighborhood Council submitted a letter in opposition to the Long Beach Port Pier B On-Dock Rail Support Facility Project (DIR-2020-7285-CDP) dated May 25, 2021.

Planning Staff has received eight (8) letters of support from the Associated General Contractors of California, Bragg Heavy Transport, California Trucking Association, Contractors Cargo Company, International Longshore and Warehouse Union, Marco Transport Inc., Rebuild SoCal Partnership, and Southern California Contractors Association. Their comments are included in Exhibit E.

EXHIBIT A

PROJECT PLANS

CPC-2020-7285-GPA

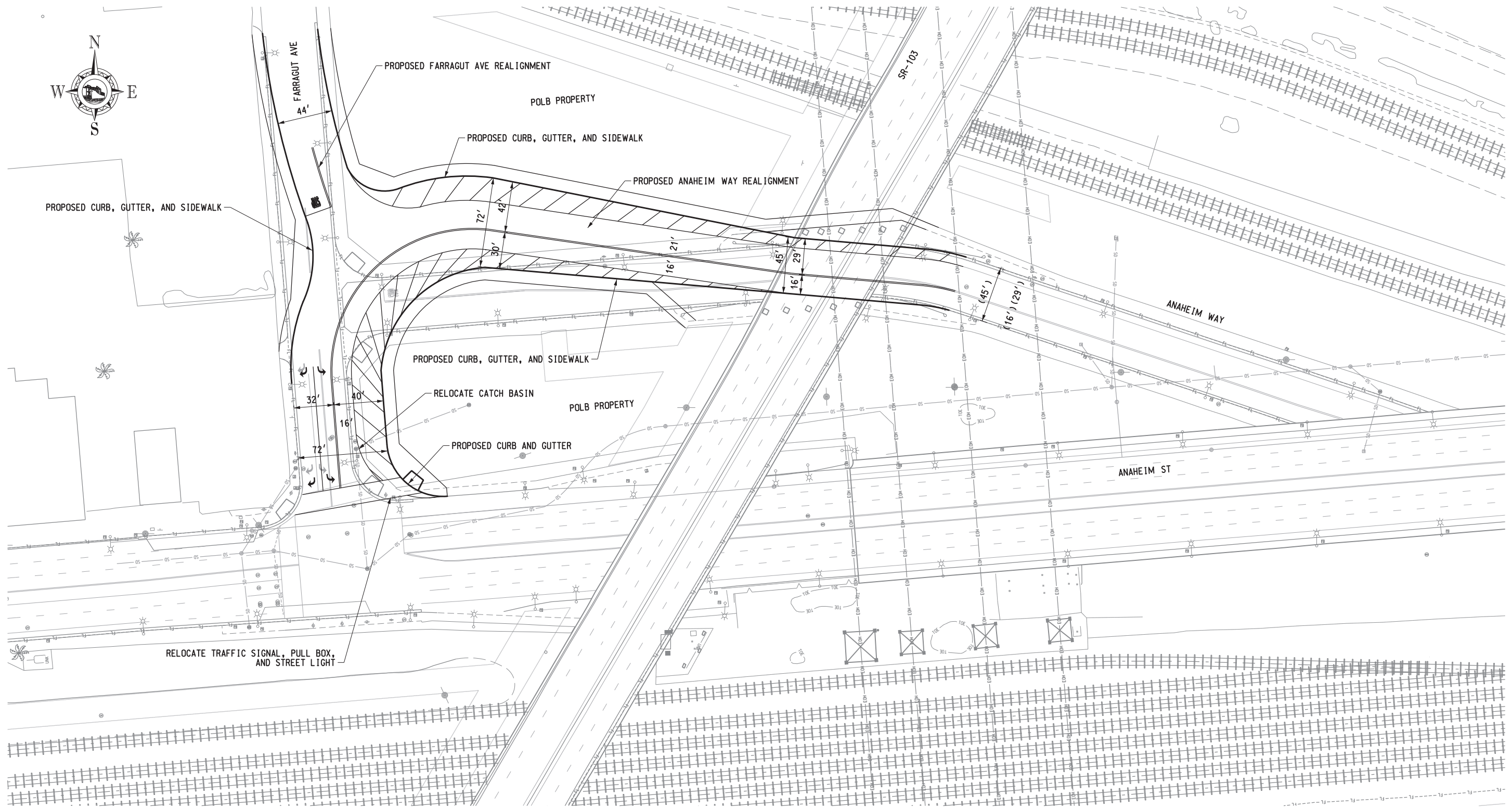


EXHIBIT 18
TRUCK TURN ANALYSIS
FARRAGUT AVE/ANAHEIM ST - SCENARIO TWO CONCEPT IMPROVEMENT PLAN

NOTICE TO CONTRACTORS

19. TRAFFIC LANE REQUIREMENTS (CONT.)

TRAFFIC CONTROL DEVICES MAINTENANCE/RESTORATION

STRIPING, SIGNAGE AND CURB MARKINGS THE B-PERMIT APPLICANT SHALL BE RESPONSIBLE FOR THE RESTORATION OF ALL OBLITERATED STRIPING AND PAVEMENT MARKINGS, AND FOR THE REINSTALLATION OF ALL MISSING, DAMAGED AND NECESSARY TRAFFIC CONTROL DEVICES (INCLUDING BUT NOT LIMITED TO SIGNS, CURB MARKINGS, SPEED HUMPS, STAMPED CROSSWALKS). THE CONTRACTOR SHALL BE REQUIRED TO PHYSICALLY INSTALL SOME OR ALL OF THE TRAFFIC CONTROL DEVICES TO THE SATISFACTION OF LADOT. THE ENTIRE COST THEREOF SHALL BE BORNE BY THE CONTRACTOR. ALL STRIPING AND PAVEMENT MARKINGS SHALL BE INSTALLED UTILIZING THERMOPLASTIC MATERIALS PER LADOT SPECIFICATIONS. THE CONTRACTOR SHALL CONTACT LADOT'S CITYWIDE INVESTIGATIONS AT (213) 928-9625 PRIOR TO COMMENCING ANY WORK THAT REQUIRES TRAFFIC CONTROL DEVICES MAINTENANCE.

TRAFFIC SIGNAL NOTES

IF THIS PROJECT INTERFERES WITH EXISTING CITY OF LOS ANGELES DEPARTMENT OF TRANSPORTATION (LADOT'S) TRAFFIC SIGNAL PULL BOXES, CONDUITS, DETECTOR LOOPS, OR OTHER EQUIPMENT; THE CONTRACTOR SHALL NOTIFY THE LADOT SIGNAL INSPECTOR THREE (3) WORK DAYS PRIOR TO BEGINNING OF CONSTRUCTION:

LADOT CENTRAL TRAFFIC SIGNAL INSPECTOR AT 213-485-1071 LADOT WESTERN TRAFFIC SIGNAL
INSPECTOR AT 213-485-6834 LADOT VALLEY TRAFFIC SIGNAL INSPECTOR AT 818-779-7433

IN THE EVENT OF DAMAGE TO EXISTING TRAFFIC SIGNAL EQUIPMENT, CONDUIT, OR DETECTOR COILS, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE LADOT TRAFFIC SIGNAL REPAIR AT (818) 752-5108, AND SHALL BE RESPONSIBLE FOR REPAIR OR REPLACEMENT AT NO COST TO THE CITY. REPAIRED OR REPLACED EQUIPMENT/CONDUIT SHALL BE INSPECTED BY THE TRAFFIC SIGNAL INSPECTOR BEFORE SIGNAL CIRCUITS ARE ENERGIZED.

NOTIFY LADOT SIGNAL MAINTENANCE SUPERVISOR AT (213) 485-7721 THREE (3) WORK DAYS PRIOR TO START OF CONSTRUCTION WHEN WITHIN 20 FEET OF TRAFFIC SIGNAL EQUIPMENT DETECTOR LOOPS, OR CONDUIT. CONTACT ATSAC FIELD IMPLEMENTATION (213) 473-8286 FOR MARKING OF ATSAC FIBER CONDUIT.

20. TREE REMOVAL:

PRIOR TO ANY STREET TREE REMOVAL, A TREE REMOVAL PERMIT APPLICATION SHALL BE SUBMITTED TO BSS URBAN FORESTRY DIVISION (BSS UFD). TREE REMOVAL PERMIT REQUIREMENTS SHALL BE DETERMINED BY THE BSS UFD. TREE REMOVAL PERMIT REQUIREMENTS SHALL BE APPROVAL BY THE BOARD OF PUBLIC WORKS WHEN REMOVING THREE OR MORE STREET TREES OR APPROVAL FROM THE CITY CHIEF FORESTER WHEN REMOVING TWO STREET TREES OR LESS. DURING THE APPLICATION PROCESS, FOR REMOVAL OF THREE OR MORE TREES, MAY TAKE FROM 90-120 DAYS DEPENDING UPON PROJECT COMPLEXITY. IF APPROVED, THE TREE REMOVAL PERMIT SHALL BE AVAILABLE AT THE JOB SITE WHILE TREE WORK IS BEING PERFORMED. IN ACCORDANCE WITH THE LOS ANGELES MUNICIPAL CODE (LAMC) SECTION 62.169, ALL TREES, REGARDLESS OF SPECIES, REQUIRE A TREE REMOVAL PERMIT. DURING THE TREE REMOVAL PERMIT APPLICATION PROCESS, BSS UFD MAY DETERMINE THAT STREET TREES DO NOT REQUIRE REMOVAL FOR COMPLETION OF THE PROJECT. ALL TREES SHALL BE PROTECTED IN PLACE UNTIL A TREE REMOVAL PERMIT HAS BEEN SECURED.

WARNING TO CONTRACTORS/PERMITEES: TREE REMOVAL WITHOUT A REMOVAL PERMIT IS A MISDEMEANOR. FAILURE TO DISCLOSE THE EXISTENCE OF A STREET TREE OR OBTAIN A TREE REMOVAL PERMIT PRIOR TO REMOVING ANY TREE MAY RENDER THE PROJECT'S CONSTRUCTION PERMIT VOID, SUBJECT THE CONTRACTOR TO FINES AND PENALTIES AS ALLOWED BY THE LAW. SUBJECT THE PROJECT SCHEDULE TO DELAYS, OR SUBJECT THE CONTRACTOR TO ANY COMBINATION OF THE ABOVE NOTED ENFORCEMENT ACTIONS.

21. TREE PRESERVATION:

A. ALL STREET TREES WITHIN THE CONSTRUCTION ZONE SHALL BE PROTECTED FROM DAMAGE. THE CONTRACTOR SHALL SUBMIT A TREE PROTECTION PLAN TO THE INSPECTOR OF PUBLIC WORKS OR THEIR ASSIGNED REPRESENTATIVE FOR REVIEW AND ACCEPTANCE; THE PLAN SHALL BE CONSISTENT WITH THE AMERICAN NATIONAL STANDARDS INSTITUTE (ANSI) A300 BEST MANAGEMENT PRACTICES. A TREE PROTECTION PLAN SHALL BE APPROVED PRIOR TO CONSTRUCTION AND KEPT ON-SITE DURING CONSTRUCTION.

























B. ROOT PRUNING OR CROWN PRUNING OR BOTH MAY BE REQUIRED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY PERMIT REQUIREMENTS AND ASSOCIATED FEES.

I. IF BSS UFD REQUIRES TREE PRESERVATION AND ROOT PRUNING, A ROOT PRUNING PERMIT SHALL BE ISSUED TO PERMITTEE. ROOT PRUNING SHALL BE PERFORMED PER THE INSTRUCTIONS CONTAINED ON THE ROOT PRUNING PERMIT.

II. IF BSS UFD REQUIRES TREE CROWN PRUNING IN ADDITION TO ROOT PRUNING, A TREE PRUNING PERMIT SHALL BE ISSUED TO THE PERMITTEE. TREE PRUNING SHALL BE IN COMPLIANCE WITH AMERICAN NATIONAL STANDARDS INSTITUTE (ANSI) A-300 GUIDELINES.

III. COPY OF THE ROOT OR CROWN PRUNING PERMIT SHALL BE AT THE JOB SITE WHILE TREE WORK IS BEING PERFORMED. ROOT OR CROWN PRUNING WITHOUT A PERMIT IS A MISDEMEANOR.

SYMBOLS



	GAS METER
	WATER METER
	WATER MANHOLE
	WATER BACKFLOW DEVICE
	WATER CHECK VALVE
	WATER FIRE HYDRANT
	WATER VALVE
	SEWER MANHOLE
	SEWER CHIMNEY
	SEWER CLEAN OUT
	SEWER LIFT STATION
	CATCH BASIN
	STORM DRAIN PUMP STATION
	STORM DRAIN MANHOLE
	ELECTRICAL PULL BOX
	LIGHT POLE
	ELECTRICAL METER
	ELECTRICAL MANHOLE
	ELECTRICAL PANEL BOX
	COMMUNICATIONS JUNCTION BOX
	COMMUNICATIONS PANEL BOX
	COMMUNICATIONS MANHOLE
	PULL BOX
	POST

EXISTING UTILITIES

Diagram illustrating the layout of a 12-inch diameter manhole with various utility lines. The manhole is represented by a circle with a cross inside. The lines are labeled as follows:

- GAS**: Grey line with 'G' markers.
- OIL**: White line with 'O' markers.
- WATER**: Blue line with 'W' markers.
- STORM DRAIN**: Green line with 'SD' markers.
- ELECTRICAL OVERHEAD**: Yellow line with 'EOH' markers.
- COMMUNICATIONS**: Orange line with 'COM' markers.





TOPOGRAPHY

 FENCE
 MIDWEST GUARDRAIL SYSTEM
 CRASH ATTENUATOR






ABBREVIATIONS

AND	OG	ORIGINAL GROUND
@	OH	OPPOSITE HAND; OVER HEAD
ABN	PB	PULL BOX
ABUT	PCC	PORTLAND CEMENT CONCRETE; POINT OF COMPOUND CURVATURE
AC	PE	POLYETHYLENE (PIPE)
ADDL	PGL	PROFILE GRADE LINE
ADJ	P.I.	POINT OF INTERSECTION OF TWO TANGENTS
AGGR	PL	PLATE; PROPERTY LINE
ALT	PP	PARTIAL PENETRATION; POWER POLE
AVE	POLB	PORT OF LONG BEACH
AVG	PRC	POINT OF REVERSE CURVATURE
BB	PT	POINT OF TANGENCY
BC	PVC	POLYVINYL CHLORIDE (PIPE); VERTICAL POINT OF CURVATURE
BLDG	PVI	POINT OF VERTICAL INTERSECTION
BLK	PVMT	PAVEMENT
BLVD	PVT	VERTICAL POINT OF TANGENCY
BRG	R	RADIUS
BVC	RCP	REINFORCED CONCRETE PIPE
CALTRANS	REINF	REINFORCE; REINFORCING
C&G	RET	RETAINING
CB	R/W	RIGHT OF WAY
CL	RR	RAILROAD
☐	Rt	RIGHT
CLB	RTL	RIGHT TURN LANE
CLR	S	SOUTH
CMB	SALV	SALVAGE
CMP	SB	SOUTHBOUND
COL	SCE	SOUTHERN CALIFORNIA EDISON
COMM	SCHED	SCHEDULE
COMP	SD	STORM DRAIN
CONC	SEC	SECTION
CONST	SF	SQUARE FEET
CU FT, CF	SHT	SHEET
CY	SHLD	SHOULDER
DBL	SL	STREET LIGHT
DIM	SP	SPUR (TRACK)
DWG	SPECS	SPECIFICATIONS
DWL	SQ	SQUARE
DWY	ST	STREET
EA	STA	STATION
EB	STD	STANDARD
EC	STR	STRUCTURAL; STRUCTURE
ECR	SW	SIDEWALK
EL or ELEV.	t	THICKNESS
ELEC	TBB	THRIE BEAM BARRIER
EP or EOP	TEMP	TEMPERATURE; TEMPORARY
EVC	TOC or TC	TOP OF CURB; TOP OF CONCRETE
EXIST	TOG	TOP OF GRATE
FG	TOR	TOP OF RAIL
FL	THRU	THROUGH
FT	TOW	TOP OF WALL
FTG	TP	TELEPHONE POLE
GB	TRK	TRACK
HP	TS	TRAFFIC SIGNAL; TUBULAR STEEL
LA	TV or T.V.	TELEVISION
LF	TYP	TYPICAL
LP	UG	UNDERGROUND
Lt	VAR	VARIABLE
LTL	VERT	VERTICAL
MAX	VC	VERTICAL CURVE
MIN	VCP	VITRIFIED CLAY PIPE
MH	W	WEST; WIDE FLANGE
N	W/	WITH
NAD	W/O	WITHOUT
NB	WB	WESTBOUND
NO. or #		
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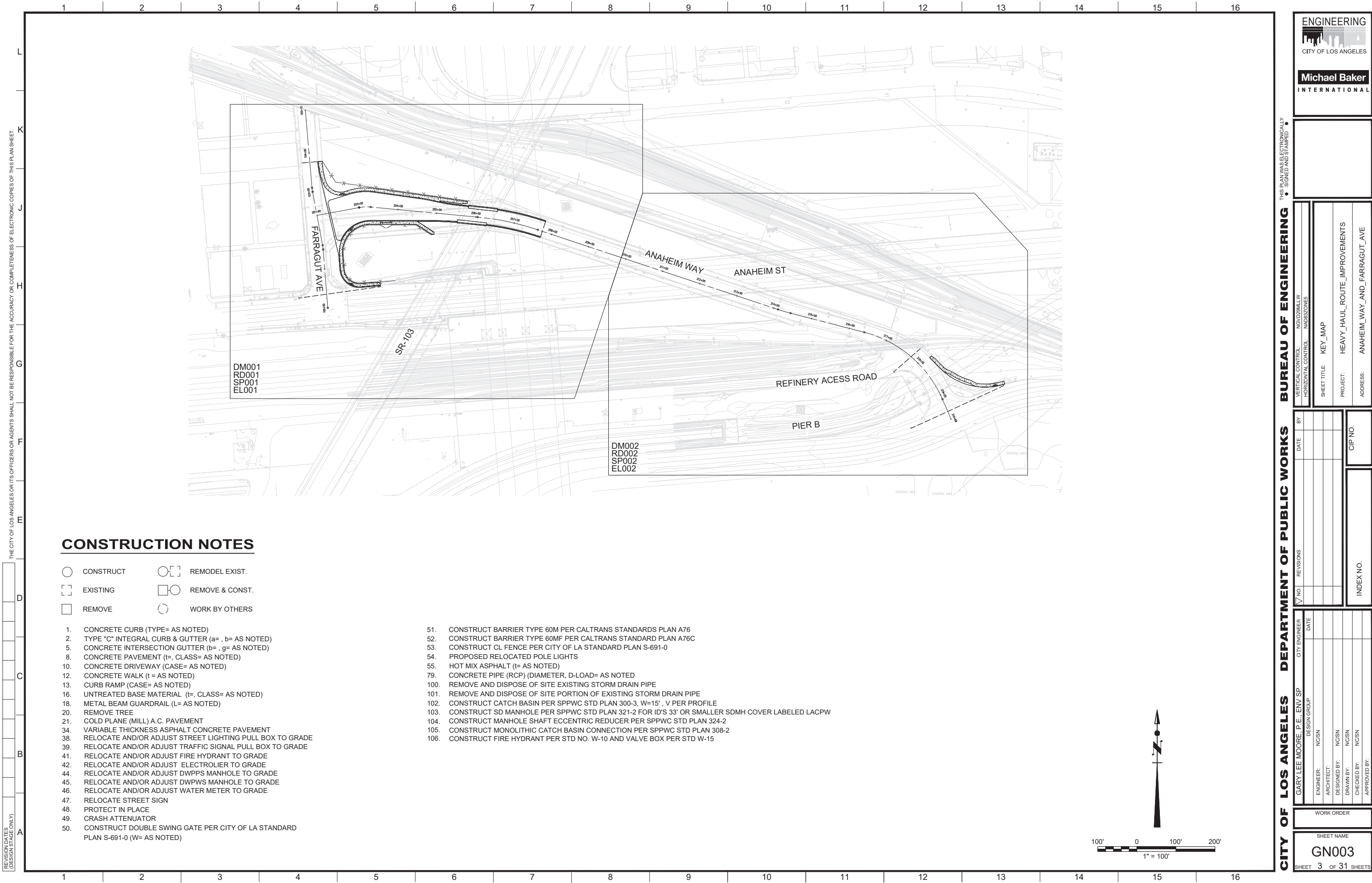
PAVEMENT

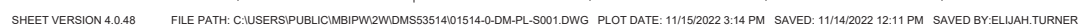
	OVERLAY ASPHALT CONCRETE
	NEW ASPHALT CONCRETE PAVEMENT
	NEW PCC PAVEMENT
	SIDEWALK

DEMOLITION

	PAVEMENT REMOVAL
	SIDEWALK REMOVAL
	C&G REMOVAL
	PCC REMOVAL
	COLD PLANE ASPHALT CONCRETE

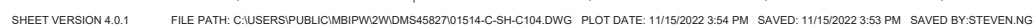


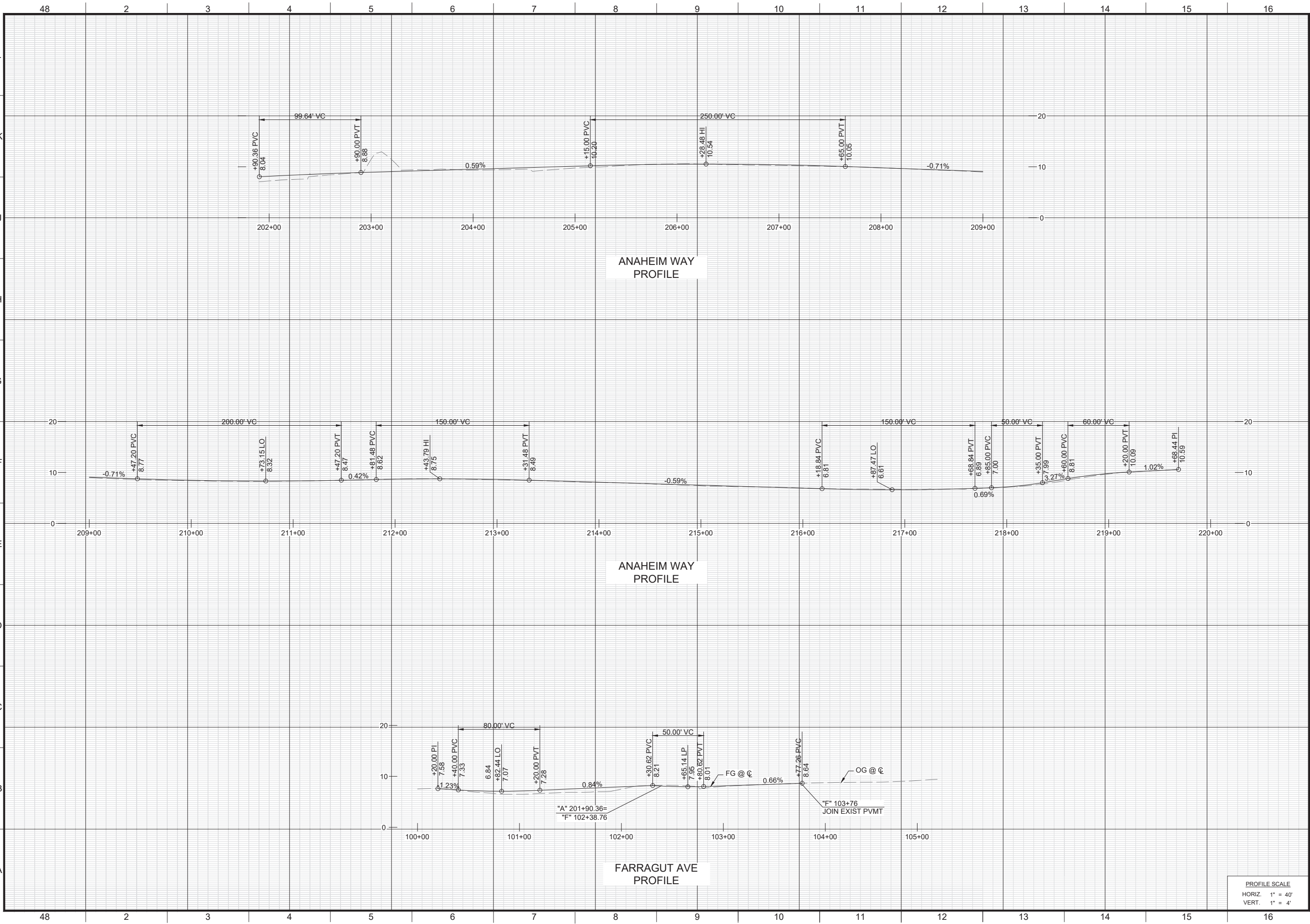














THIS PLAN WAS ELECTRONICALLY
SIGNED AND STAMPED

BUREAU OF ENGINEERING

VERTICAL CONTROL:	NGVD29MLLW
HORIZONTAL CONTROL:	NAD83ZONE5

SHEET TITLE: ROADWAY_DETAILS

PROJECT: HEAVY_HAUL_ROUTE_IMPROVEMENTS

ADDRESS: ANAHEIM WAY AND FARRAGUT AVE

CIP NO. XXXXX

D-XXXXXX

INDEX NO.







GARY LEE MOORE, P.E., ENV SP CITY ENGINEER

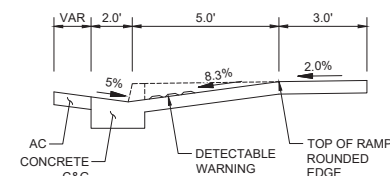
WORK ORDER
XXX#####

SHEET NAME
RD005

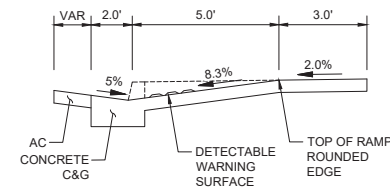
SHEET 10 OF 31 SHEET

CONSTRUCTION NOTES

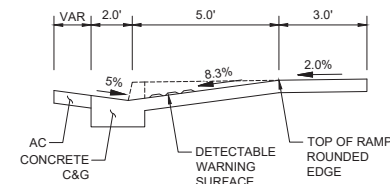
- | | | | |
|---|-----------|---|-----------------|
|  | CONSTRUCT |  | REMODEL EXIST. |
|  | EXISTING |  | REMOVE & CONST. |
|  | REMOVE |  | WORK BY OTHERS |
-
- | | |
|-----|--|
| 10. | CONCRETE DRIVEWAY (CASE= AS NOTED) |
| 12. | CONCRETE WALK (t = AS NOTED) |
| 13. | CURB RAMP (CASE= AS NOTED) |
| 20. | REMOVE TREE |
| 50. | CONSTRUCT DOUBLE SWING GATE PER CITY OF LA
STANDARD S-691-0 (W= AS NOTED) |



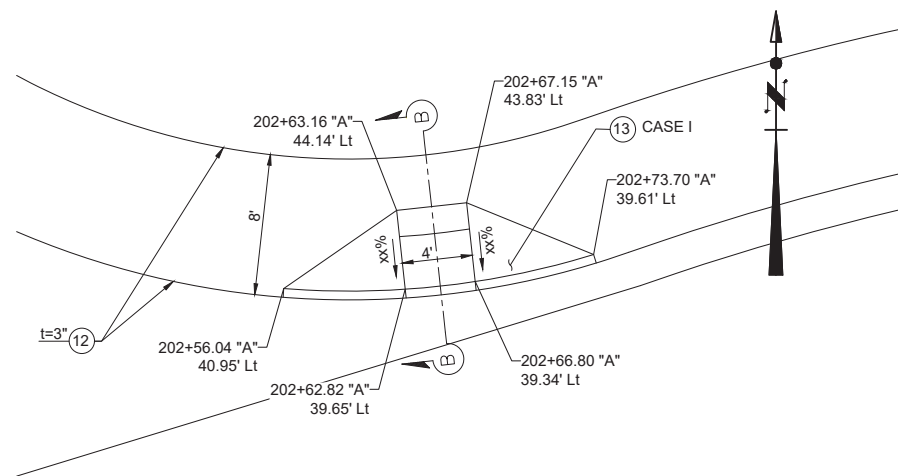
SECTION A-A
NOT TO SCALE



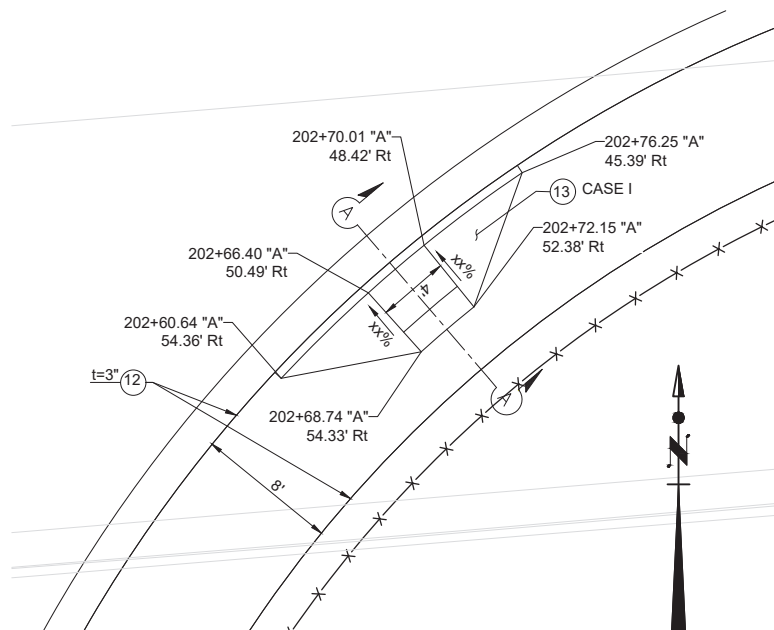
SECTION B-B
NOT TO SCALE



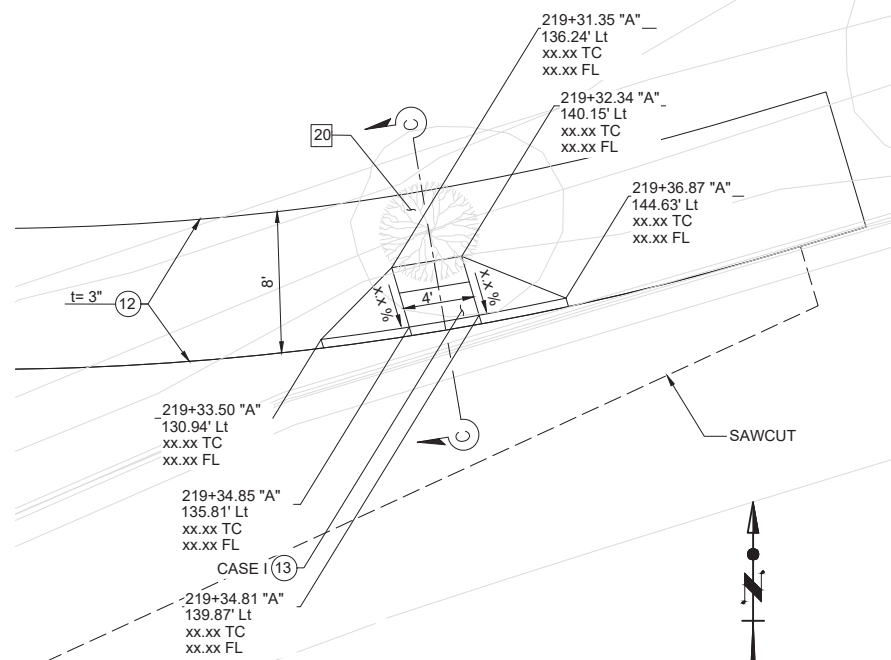
SECTION C-C
NOT TO SCALE



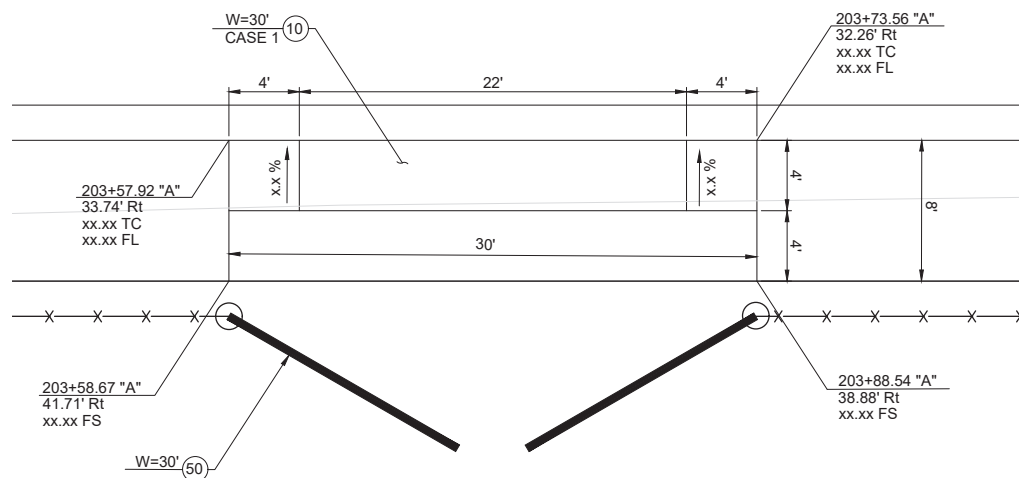
ANAHEIM WAY / FARRAGUT AVENUE
NORTH CURB RAMP DETAIL
SCALE: 1" = 5'



ANAHEIM WAY / FARRAGUT AVENUE
SOUTH CURB RAMP DETAIL
SCALE: 1" = 5'



ANAHEIM WAY / PIER B STREET
CURB RAMP DETAIL
SCALE: 1" = 5'

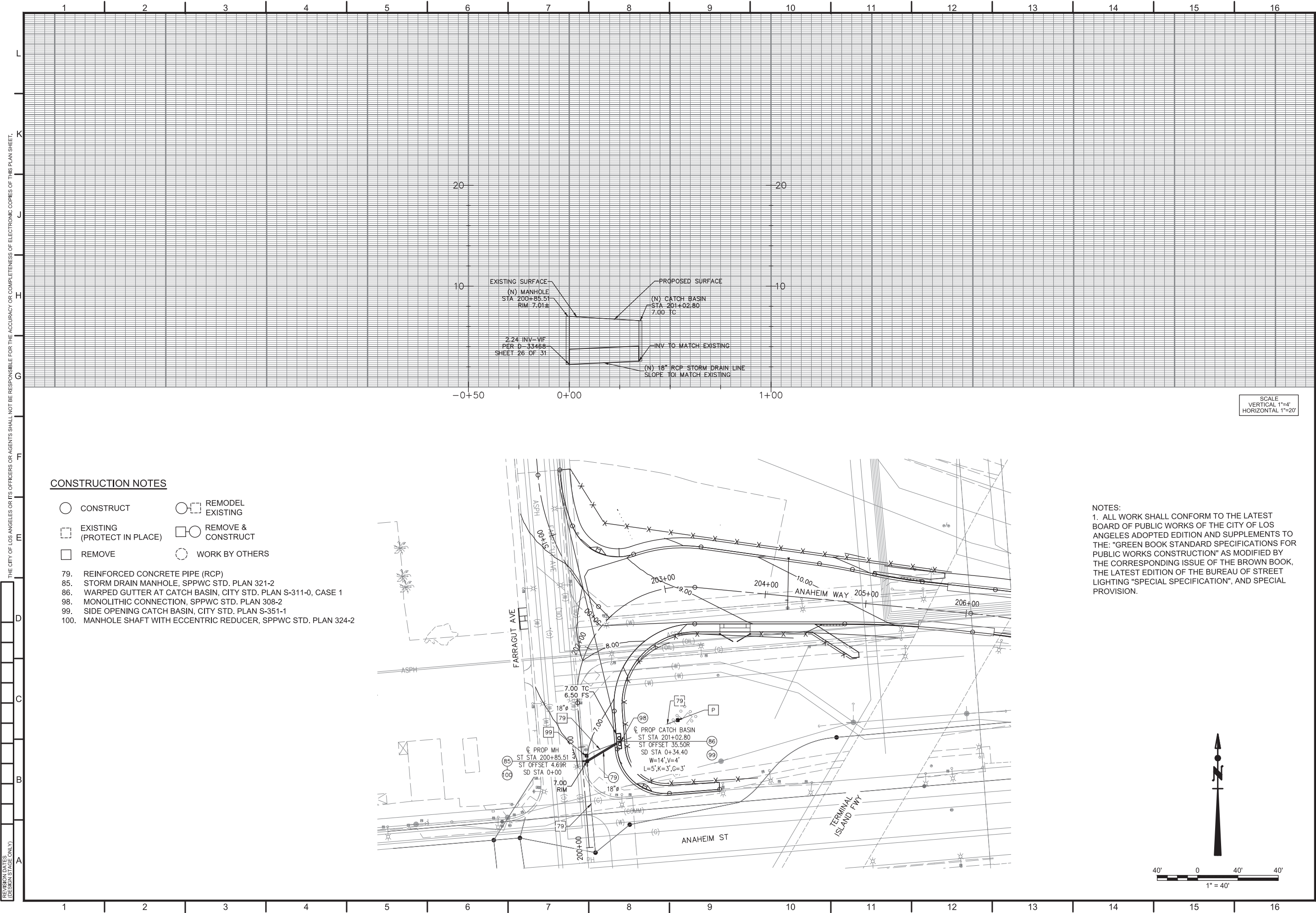


DRIVEWAY DETAIL "A"
203+73.32
SCALE: 1" = 5'

THE CITY OF LOS ANGELES OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.

REVISION DATES
(DESIGN STAGE ONLY)

SHEET VERSION 4.0.1 FILE PATH: C:\USERS\PUBLIC\MBIPW2\WDMS45821\01514-C-SH-C507.DWG PLOT DATE: 11/15/2022 3:29 PM SAVED: 11/14/2022 3:25 PM SAVED BY: ELIJAH.TURNER



ENGINEERING
CITY OF LOS ANGELES

JMC²
Civil Engineering & Surveying
John M. Chisholm & Associates, Inc.
4111 N. Harbor Blvd., Suite 201
San Pedro, CA 90731
P: 310.341.6850
www.jmc2.com

PROFESSIONAL ENGINEER
John M. Chisholm
Exp. 6-30-22
No. 57815
STATE OF CALIFORNIA

THIS PLAN WAS ELECTRONICALLY SIGNED AND STAMPED

BUREAU OF ENGINEERING

VERTICAL CONTROL: _____
HORIZONTAL CONTROL: _____

SHEET TITLE: _____
PROJECT: _____
ADDRESS: _____

DEPARTMENT OF PUBLIC WORKS

DATE: _____ BY: _____
REVISIONS: _____

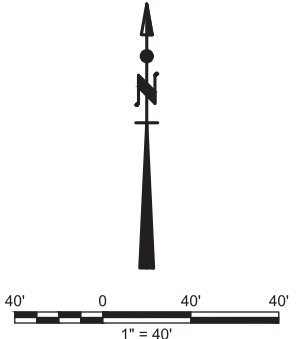
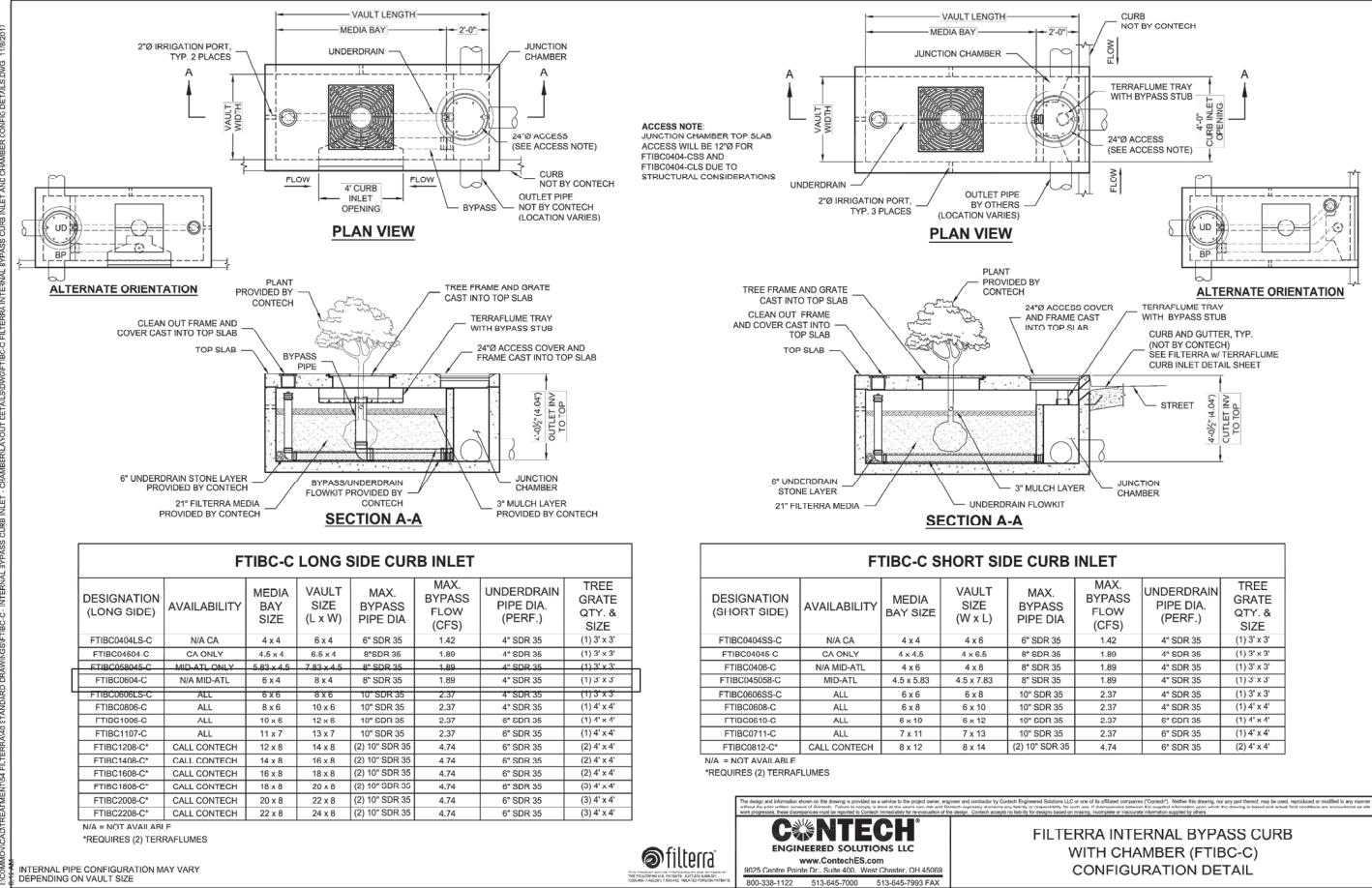
CIP NO. _____
INDEX NO. _____

CITY OF LOS ANGELES

GARY LEE MOORE, P.E., ENV SP
DESIGN GROUP: _____
ENGINEER: JOSMAR ORTIZ, EIT
ARCHITECT: _____
DESIGNED BY: JOSMAR ORTIZ, EIT
DRAWN BY: JOSMAR ORTIZ, EIT
CHECKED BY: KHALED ABDOL PE
APPROVED BY: _____

WORK ORDER _____

SHEET NAME
SD001
SHEET 11 OF 31 SHEETS





SEE SHEET SP003 FOR GENERAL STRIPING & SIGNING NOTES,
STRIPING & SIGNING CONSTRUCTION NOTES, AND LEGEND.



BUREAU OF ENGINEERING

VERTICAL CONTROL:	NGVD29MLLW
HORIZONTAL CONTROL:	NAD83ZONE5

SHEET TITLE: SIGNING_AND_STRIPING_PLAN_01

PROJECT:	HEAVY_HAUL_ROUTE_IMPROVEMENTS
----------	-------------------------------

ADDRESS: ANAHEIM_WAY_AND_FARRAGUT_AVE

DEPARTMENT OF PUBLIC WORKS

CITY ENGINEER	NO.	REVISIONS
---------------	-----	-----------

CITY ENGINEER

GARY LEE MOORE, P.E., ENV SP

ENGINEER:	AD
-----------	----

ARCHITECT:	AD
------------	----

DRAWN BY:	AD
CHECKED BY:	MV

APPROVED BY:

WORK ORDER

SHEET NAME

SP001

SHEET 18 OF 31 SHEETS

GENERAL STRIPING NOTES

1. TRAFFIC STRIPING, MARKINGS AND MARKERS SHALL CONFORM TO LATEST EDITION OF CALTRANS STANDARD PLANS, CALTRANS STANDARD SPECIFICATIONS, AND CALIFORNIA MUTCD.
2. ALL STRIPING LINES AND PAVEMENT MARKINGS SHALL BE THERMOPLASTIC MATERIAL.
3. ALL EXISTING CONFLICTING STRIPING, ARROWS, OR PAVEMENT MARKINGS SHALL BE REMOVED BY GRINDING PRIOR TO FINAL STRIPING INSTALLATION. ALL EXISTING STRIPING THAT IS REMOVED OR DAMAGED BY NEW CONSTRUCTION SHALL BE REPLACED IN KIND.
4. PROCEDURES FOR APPLICATION OF STRIPING, PAVEMENT MARKINGS AND MARKERS SHALL CONFORM TO CALTRANS STANDARD SPECIFICATIONS: SECTION 84-2.
5. LANE WIDTHS SHALL BE MEASURED BETWEEN THE CENTERLINES OF EACH ADJACENT SINGLE OR DOUBLE STRIPE OR TOP OF CURB AS APPROPRIATE.
6. ALL NEW STRIPING SHALL BE CAT-TRACKED AND APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AFTER COMPLETING LAYOUT AT LEAST 3 WORKING DAYS BEFORE COMMENCING INSTALLATION OF STRIPING, MARKINGS, AND MARKERS.
7. THE CONTRACTOR SHALL FURNISH TO THE ENGINEER SAMPLES OF MATERIALS NOT LESS THAN 3 WEEKS IN ADVANCE OF THE DATE THE MATERIALS ARE TO BE APPLIED.
8. STRIPING MATERIALS SHALL CONFORM TO THE CALTRANS LIST OF PREQUALIFIED AND TESTED DELINEATION MATERIALS. SEE: http://www.dot.ca.gov/hq/esc/approved_products_list/pdf/signing_and_delineation_materials.pdf.
9. AT LOCATIONS WITH A ROADWAY SURFACE OF PORTLAND CONCRETE CEMENT, PRIOR TO APPLYING WHITE LANE LINE STRIPING, THE CONTRACTOR SHALL APPLY A BLACK STRIPE BENEATH THE PROPOSED STRIPING LOCATIONS.
10. DURING CONSTRUCTION CONTRACTOR SHALL PROVIDE TEMPORARY TRAFFIC CONTROL THAT MEETS THE STANDARDS AND REQUIREMENTS SET FORTH BY THE CA MUTCD, PART 6, AND THE ENGINEER. THE CONTRACTOR SHALL UTILIZE, BUT NOT BE LIMITED TO THE FOLLOWING FIGURES FROM THE CA MUTCD:

FIGURE 6C-2, TYPES OF TAPERS AND BUFFER SPACES
FIGURE 6H-3, WORK ON THE SHOULDERS (TA-3)
FIGURE 6H-10, LANE CLOSURE ON TWO-LANE ROAD USING FLAGGERS (TA-10)
FIGURE 6H-46, WORK IN THE VICINITY OF A GRADE CROSSING (TA-46)
FIGURE 6H-105(CA), LANE SHIFT ON ROAD WITH LOW TRAFFIC VOLUMES (TA-105(CA))
11. ALL LANE LINE AT INTERSECTION APPROACHES AND DEPARTURES SHALL BEGIN AND END WITH 50 FEET OF 6-INCH SOLID WHITE LINE.
12. THE CONTRACTOR SHALL INSTALL BLUE PAVEMENT MARKERS ADJACENT TO ALL FIRE HYDRANTS PER CA MUTCD FIGURE 38-102(CA).

GENERAL SIGNING NOTES

1. ALL NEW SIGNING SHALL CONFORM TO THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION AND SHALL MEET THE LATEST RETRO-REFLECTIVITY REQUIREMENTS.
2. ALL SIGNING SHOWN HEREON SHALL BE INSTALLED, RELOCATED, REMOVED, OR PROTECTED IN PLACE BY THE CONTRACTOR, UNLESS OTHERWISE NOTED.
3. THE CONTRACTOR SHALL FURNISH TO THE ENGINEER SAMPLES OF MATERIALS NOT LESS THAN 3 WEEKS IN ADVANCE OF THE DATE THE MATERIALS ARE TO BE APPLIED.
4. SIGNS SHALL BE STANDARD SIZE UNLESS NOTED OTHERWISE PRIOR TO INSTALLATION. NEW SIGNS SHALL BE APPROVED BY THE ENGINEER. EXACT LOCATION OF SIGNS SHALL BE DETERMINED BY THE ENGINEER.
5. ALL SIGNS SHALL BE HIGH INTENSITY GRADE REFLECTIVE WITH PREMIUM PROTECTIVE OVERLAY.
6. CONTRACTOR SHALL INSTALL SIGNS ON EXISTING STREET LIGHT POLES WHEN DEEMED ACCEPTABLE BY THE ENGINEER. WHEN DEEMED NECESSARY TO INSTALL A POST WITH THE SIGN, THE POST SHALL BE "TELESPAR QUIK-PUNCH," OR APPROVED EQUAL, WITH RECEPTIVE 30-INCH OR 36-INCH ANCHOR ASSEMBLY.
7. FOR NON-STANDARD SIGNS, THE CONTRACTOR SHALL PROVIDE A TO SCALE PAPER COPY OF THE SIGN PANEL, 3 WEEKS BEFORE PROCUREMENT.
8. SIGNING MATERIALS SHALL CONFORM TO THE CALTRANS LIST OF PREQUALIFIED AND TESTED SIGNING MATERIALS. SEE: [HTTP://WWW.DOT.CA.GOV/HQ/ESC/APPROVED_PRODUCTS_LIST/PDF/SIGNING_AND_DELINEATION_MATERIALS.PDF](http://www.dot.ca.gov/hq/esc/approved_products_list/pdf/signing_and_delineation_materials.pdf).




STRIPING CONSTRUCTION NOTES

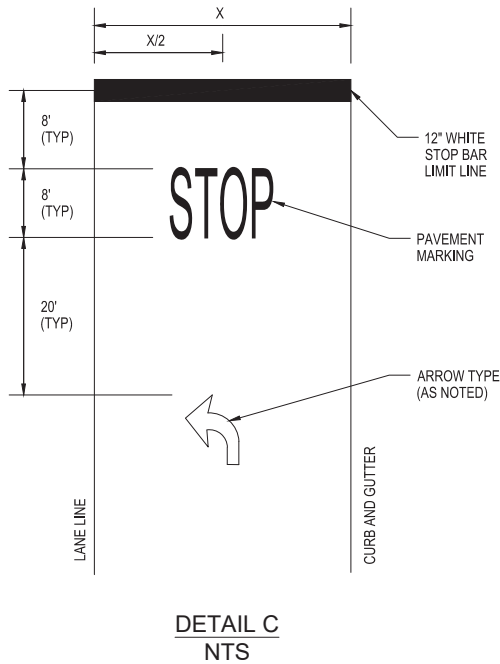
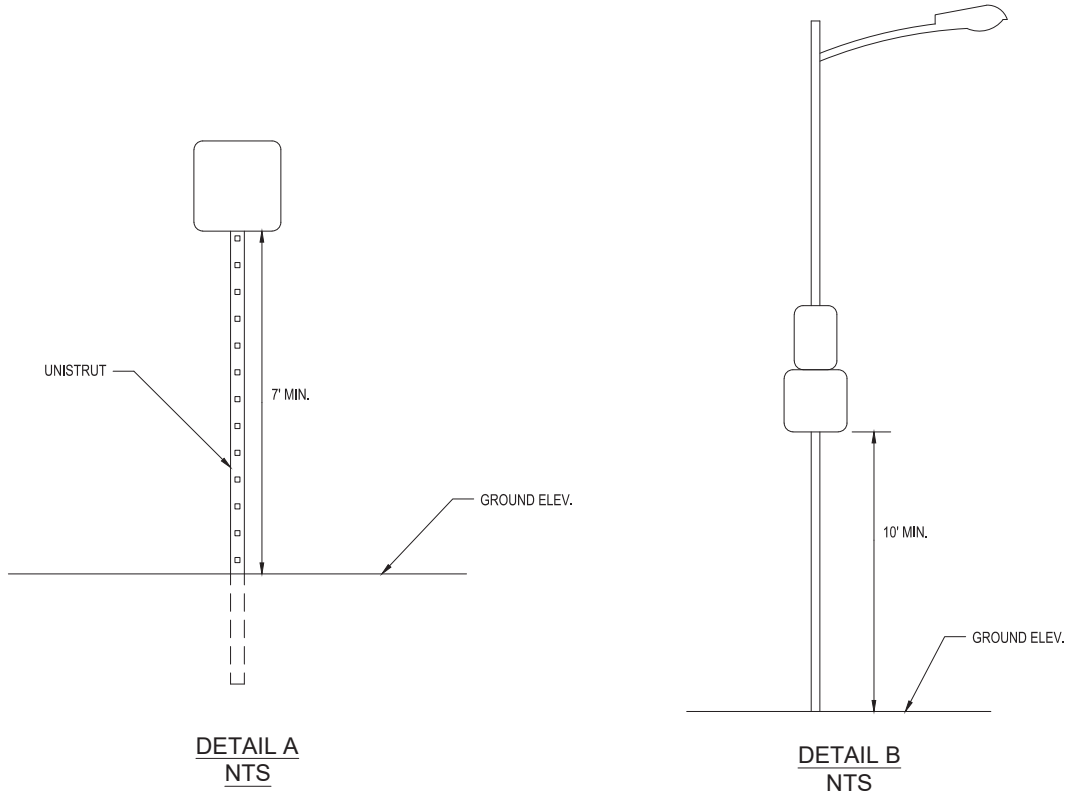
- | | |
|-----|---|
| 6 | INSTALL 6" YELLOW CENTERLINE PER CALTRANS STANDARD PLAN RSP A20A, DETAIL 6. |
| 22 | INSTALL 6" YELLOW NO PASSING ZONE-TWO DIRECTION PER CALTRANS STANDARD PLAN RSP A20A, DETAIL 22. |
| 27B | INSTALL 6" WHITE RIGHT EDGELINE PER CALTRANS STANDARD PLAN RSP A20B, DETAIL 27B. |
| 28 | INSTALL 6" DOUBLE YELLOW MEDIAN ISLAND PER CALTRANS STANDARD PLAN RSP A20B, DETAIL 28. |
| 29 | INSTALL 6" DOUBLE YELLOW MEDIAN ISLAND PER CALTRANS STANDARD PLAN RSP A20B, DETAIL 29. |
| 32 | INSTALL 6" DOUBLE TWO-WAY LEFT TURN PER CALTRANS STANDARD PLAN RSP A20B, DETAIL 32. |
| 38 | INSTALL 8" WHITE CHANNELIZING LINE PER CALTRANS STANDARD PLAN RSP A20D, DETAIL 38. |
| PM | INSTALL PAVEMENT MARKING OR ARROW AS NOTED PER CALTRANS STANDARD PLANS RSP A20A THROUGH A24F. |
| LL | INSTALL 12" WHITE LIMIT LINE OR CROSSWALK AS NOTED PER CALTRANS STANDARD PLANS A24F AND A24G. |
| RM | REMOVE EXISTING CONFLICTING STRIPING. |

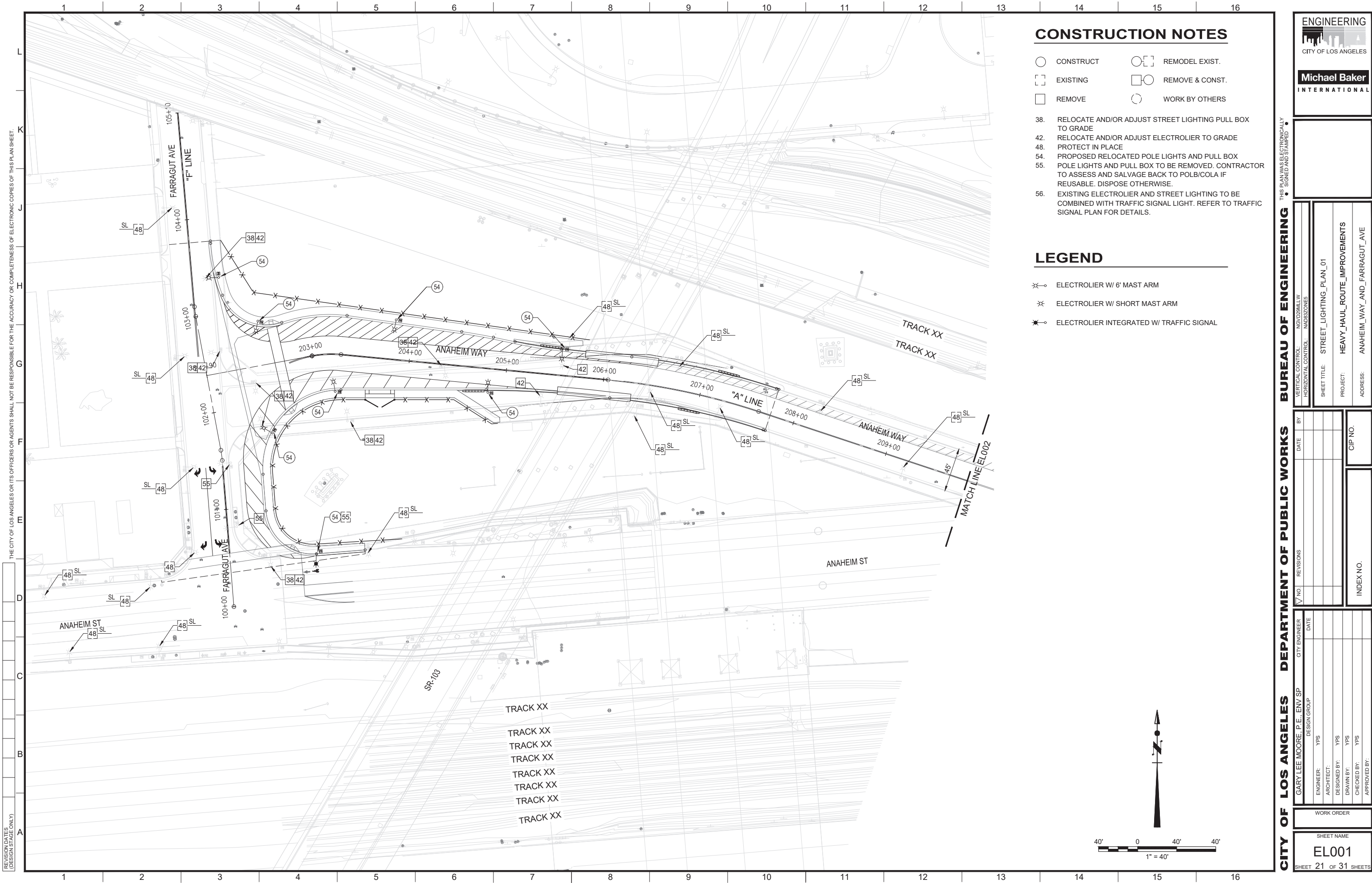
SIGNAGE CONSTRUCTION NOTES

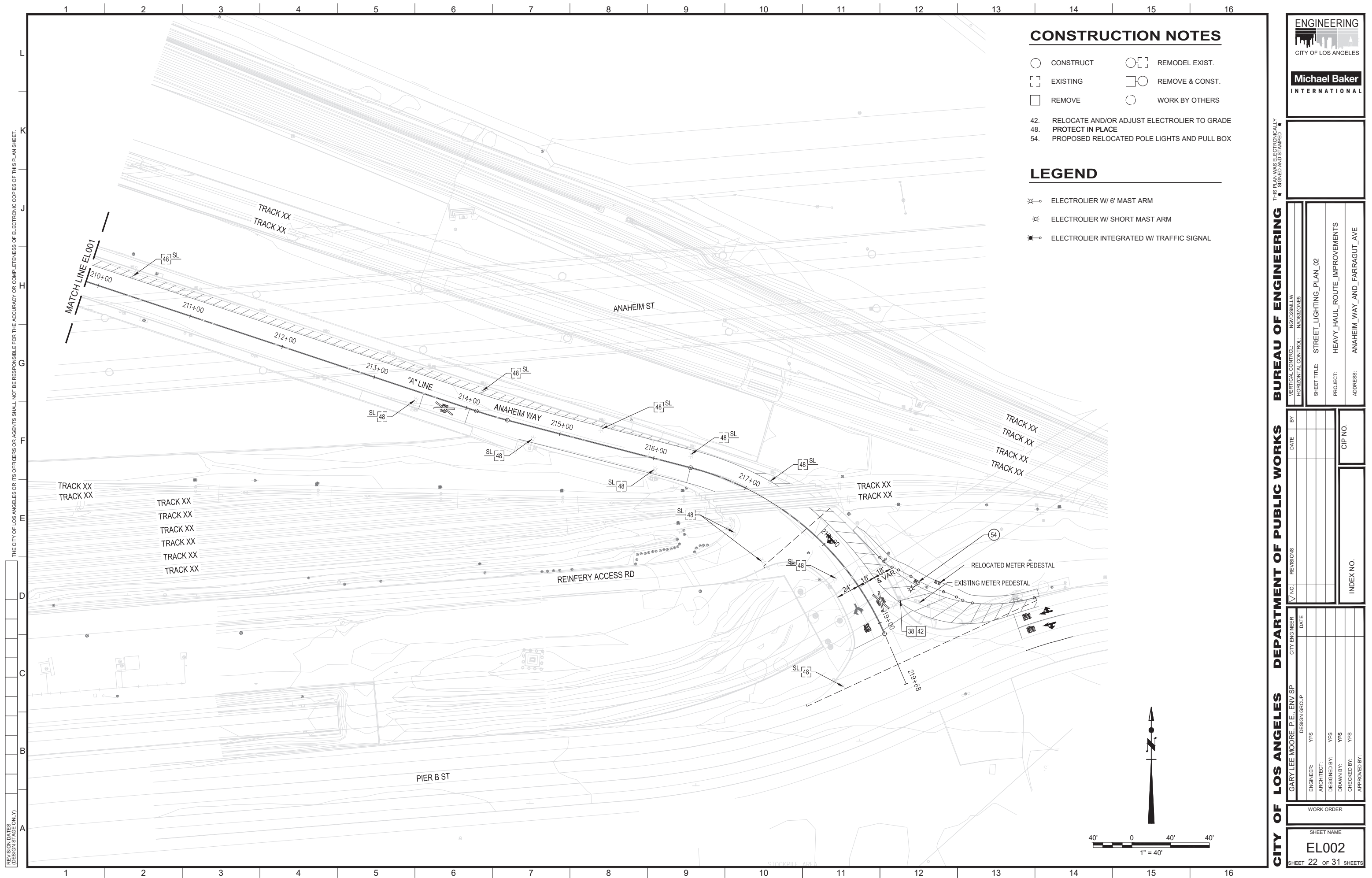
- | | |
|----|--|
| RS | REMOVE EXISTING SIGN(S) (TYPE AS NOTED PER PLAN) AND EXISTING POST AS NECESSARY PER PLAN. |
| IS | FURNISH AND INSTALL SIGN(S), TYPE AS NOTED PER PLANS, REFER TO DETAILS SHOWN BELOW. |
| E | PROTECT IN PLACE EXISTING SIGN(S), TYPE AS NOTED PER PLAN, (CITY FURNISHED SIGN BARCODE AT THE LOWER RIGHT CORNER ON THE BACK SIGN PANEL). |
| | RELOCATE EXISTING SIGN(S) (IF APPLICABLE) AS NOTED. |

LEGEND

- | | |
|---|--|
| XX' | LANE DIMENSION |
|  | CHANGE IN PAVEMENT DELINEATION DETAIL. |
|  | NEW SIGN ON EXISTING OR NEW SINGLE POST, AS SHOWN ON PLAN. |
|  | EXISTING SIGN AND POST. |
| | EXISTING SIGN ON EXISTING STREET LIGHT POLE. |
| | PROPOSED SIGN ON PROPOSED STREET LIGHT POLE. |
| SNS - STREET NAME SIGN | |







THIS PLAN WAS ELECTRONICALLY
SIGNED AND STAMPED

BUREAU OF ENGINEERING

VERTICAL CONTROL:	NGVD20MILLW
HORIZONTAL CONTROL:	NAD83_ZONES
SHEET TITLE: LANDSCAPE_CONCEPT	
PROJECT:	HEAVY_HAUL_ROUTE_IMPROVEMENTS
ADDRESS:	ANAHEIM_WAY

<input checked="" type="checkbox"/> NO.	REVISIONS	DATE	BY

CIP NO.

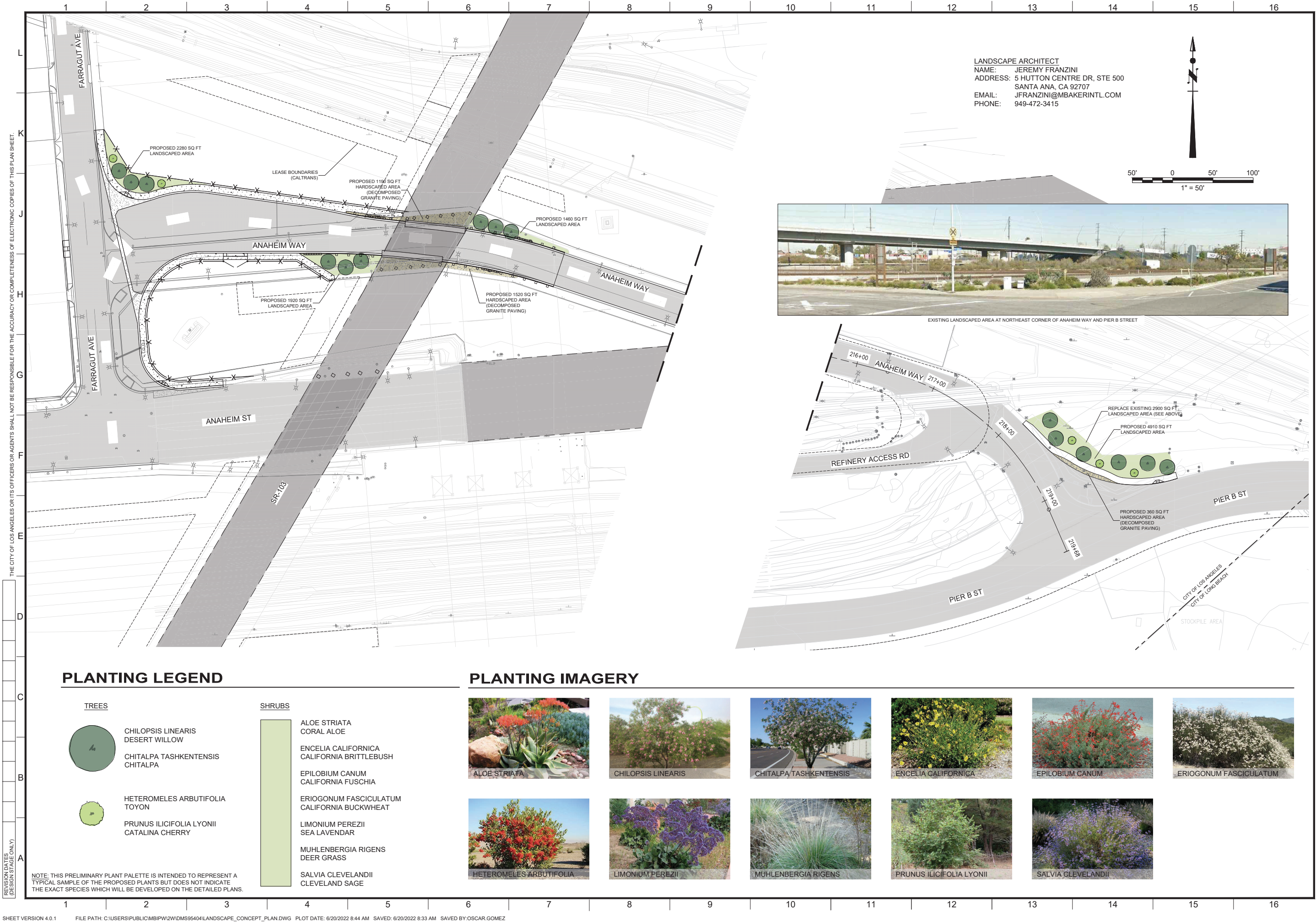
INDEX NO.

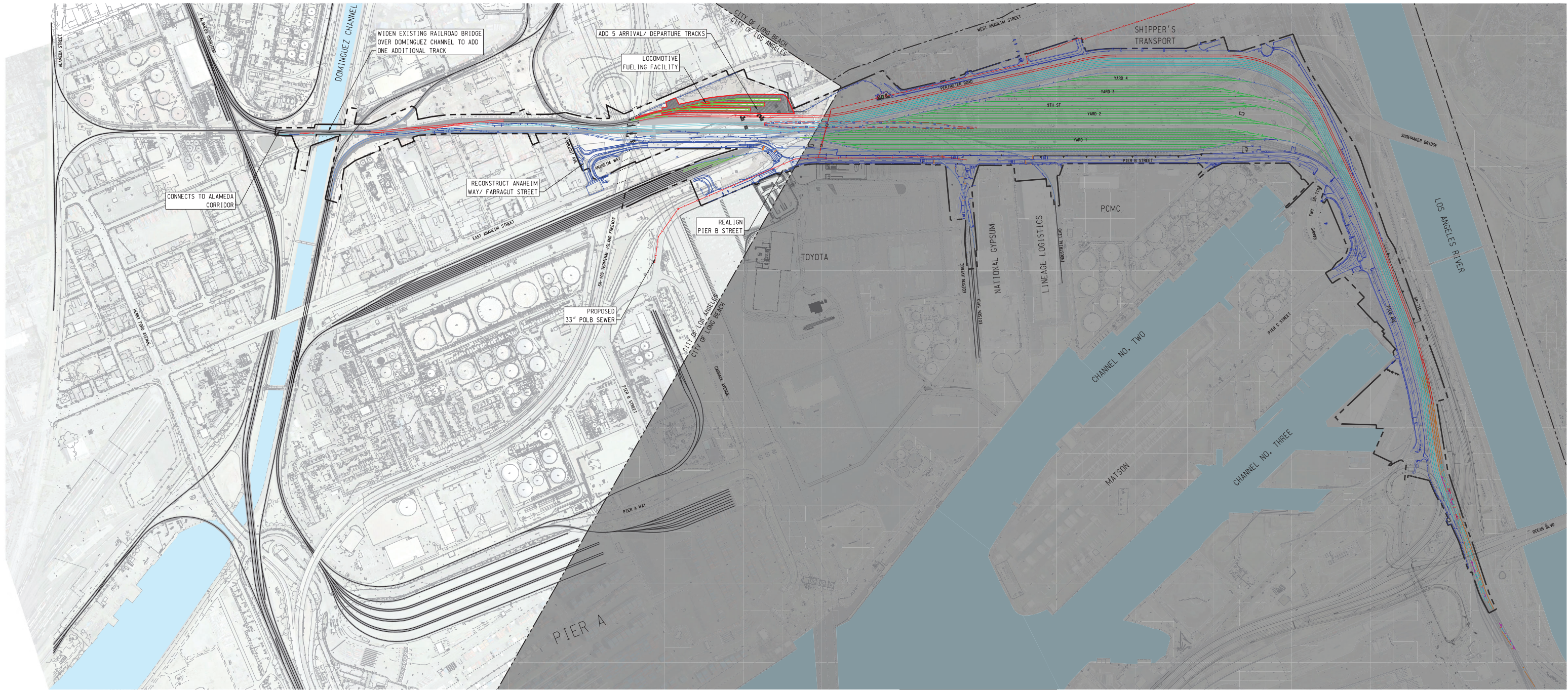
GARY LEE MOORE, P.E., ENV SP			CITY ENGINEER
DESIGN GROUP		DATE	
ENGINEER:	NOET		
ARCHITECT:	JF		
DESIGNED BY:	OCJF		
DRAWN BY:	OCJET		
CHECKED BY:	NOET		
APPROVED BY:			

WORK ORDER

SHEET NAME

SHEET	OF	SHEETS
-------	----	--------

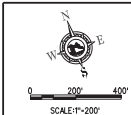




LEGEND

- PROJECT BOUNDARY
- PROPOSED ROADWAY
- PROPOSED MAINLINE TRACK
- PROPOSED RECEIVING/DEPARTURE TRACK
- PROPOSED STORAGE TRACK
- PROPOSED ANCILLARY TRACK
- PROPOSED 4TH TRACK AT OCEAN BLVD PROJECT

ASPHALT ACCESS ROAD



PORT OF LONG BEACH
ON-DOCK RAIL SUPPORT FACILITY PROGRAM
PROJECT SITE PLAN

SHEET
06 OF 08
LAST UPDATED
3-25-2022

EXHIBIT B

MAPS AND PHOTOS

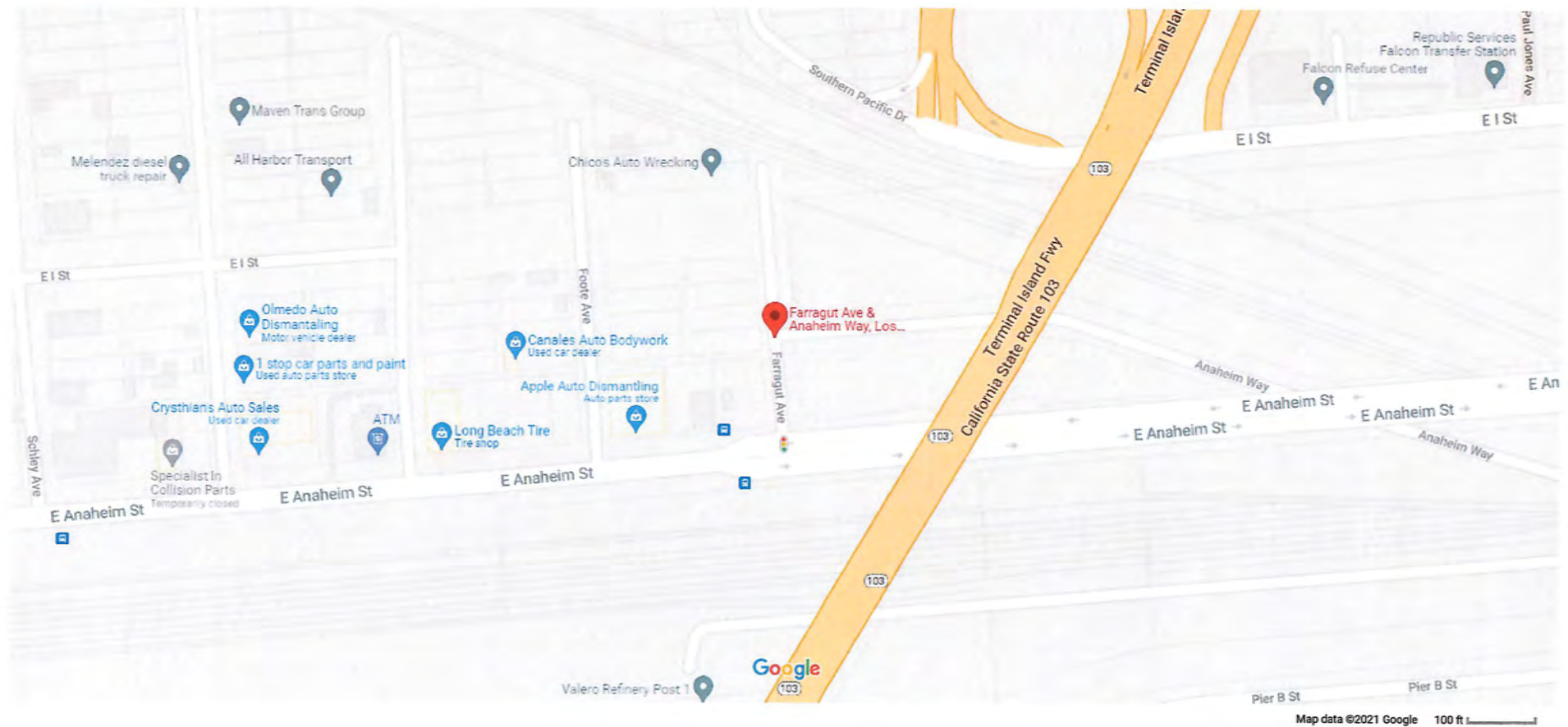
B1 – Vicinity Map

B2 – Radius Map

B3 – ZIMAS Parcel Profile Report


B4 – Site Photos

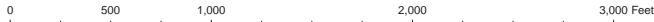
Google Maps Anaheim Way & Farragut Ave



LMG
Leon Mapping & GIS Services
15031 Chatham St., Ste 17
Mission Hills, CA 91345
818-235-7649
leonmapping@hotmail.com
www.lmradmaps.com

LEGAL DESC. SEE APPLICATION				G.P.A. 500' RADIUS	
Community Plan Area - Wilmington - Harbor City Area Planning Commission - Harbor Neighborhood Council - Wilmington Council District - CD 15 - Joe Buacino Census Tract # 9800.14 LADBS District Office - San Pedro	Thomas Brothers Grid PAGE 794-H5 PAGE 795-A5 PAGE 794-H6 PAGE 795-A6 PAGE 794-H7 PAGE 795-A7 PAGE 794-J5 PAGE 794-J6 PAGE 794-J7	<u>Map Sheet</u>		CASE #	ACREAGE: ± 110 AC
		033B213		USGS: FIELD	DATE: 1/3/2023
		033B217			UPDATE: _____
		030B213			
		030B217			


N



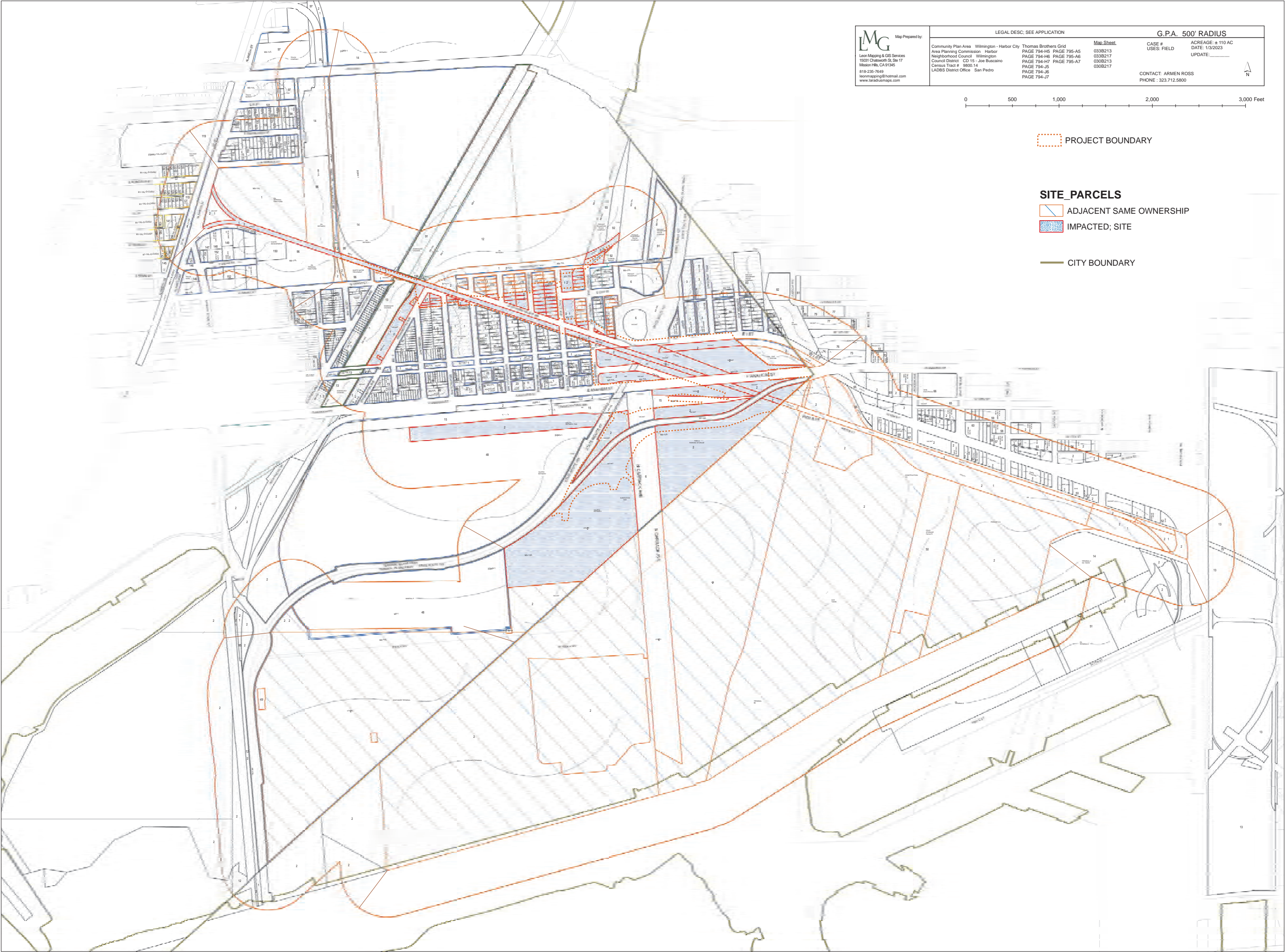
PROJECT BOUNDARY

SITE PARCELS

ADJACENT SAME OWNERSHIP

IMPACTED; SITE

CITY BOUNDARY





City of Los Angeles Department of City Planning

9/21/2021

PARCEL PROFILE REPORT

PROPERTY ADDRESSES

None

ZIP CODES

None

RECENT ACTIVITY

None

CASE NUMBERS

CPC-2018-6402-CPU
CPC-2010-1238-CRA
CPC-2005-8252-CA
CPC-1986-833-GPC
ORD_162741
ORD-85015-OD6
ORD-167242
ORD-166001
ORD-164960
ORD-163420
ENV-2019-3379-EIR
ENV-2017-2502-CE
ENV-2005-8253-ND

Address/Legal Information

PIN Number	033B217 497
Lot/Parcel Area (Calculated)	15,117.2 (sq ft)
Thomas Brothers Grid	PAGE 794 - GRID J6
Assessor Parcel No. (APN)	7428014BRK
Tract	DOMINGUEZ HARBOR TRACT
Map Reference	M B 12-14/15
Block	None
Lot	LOT E
Arb (Lot Cut Reference)	None
Map Sheet	033B217

Jurisdictional Information

Community Plan Area	Wilmington - Harbor City
Area Planning Commission	Harbor
Neighborhood Council	Wilmington
Council District	CD 15 - Joe Buscaino
Census Tract #	9800.14
LADBS District Office	San Pedro

Planning and Zoning Information

Special Notes	None
Zoning	M3-1VL
Zoning Information (ZI)	ZI-1192 Border Zone Property: 2000 ft. Buffer Zone (420 Henry Ford Avenue) ZI-2427 Freeway Adjacent Advisory Notice for Sensitive Uses ZI-2498 Local Emergency Temporary Regulations - Time Limits and Parking Relief - LAMC 16.02.1 ZI-2130 State Enterprise Zone: Harbor Gateway
General Plan Land Use	Heavy Manufacturing
General Plan Note(s)	Yes
Hillside Area (Zoning Code)	No
Specific Plan Area	None
Subarea	None
Special Land Use / Zoning	None
Historic Preservation Review	No
Historic Preservation Overlay Zone	None
Other Historic Designations	None
Other Historic Survey Information	None
Mills Act Contract	None
CDO: Community Design Overlay	None
CPIO: Community Plan Imp. Overlay	None
Subarea	None
CUGU: Clean Up-Green Up	None
HCR: Hillside Construction Regulation	No
NSO: Neighborhood Stabilization Overlay	No
POD: Pedestrian Oriented Districts	None
RFA: Residential Floor Area District	None
RIO: River Implementation Overlay	No
SN: Sign District	No

This report is subject to the terms and conditions as set forth on the website. For more details, please refer to the terms and conditions at zimas.lacity.org
(*) - APN Area is provided "as is" from the Los Angeles County's Public Works, Flood Control, Benefit Assessment.

Streetscape	No
Adaptive Reuse Incentive Area	None
Affordable Housing Linkage Fee	
Residential Market Area	Low
Non-Residential Market Area	Low
Transit Oriented Communities (TOC)	Not Eligible
RPA: Redevelopment Project Area	None
Central City Parking	No
Downtown Parking	No
Building Line	None
500 Ft School Zone	No
500 Ft Park Zone	No
Assessor Information	
Assessor Parcel No. (APN)	7428014906
APN Area (Co. Public Works)*	0.000 (ac)
Use Code	8100 - Miscellaneous - Utility
Assessed Land Val.	\$0
Assessed Improvement Val.	\$0
Last Owner Change	08/18/2020
Last Sale Amount	\$0
Tax Rate Area	14
Deed Ref No. (City Clerk)	None
Building 1	No data for building 1
Building 2	No data for building 2
Building 3	No data for building 3
Building 4	No data for building 4
Building 5	No data for building 5
Rent Stabilization Ordinance (RSO)	No [APN: 7428014906]
Additional Information	
Airport Hazard	None
Coastal Zone	None
Farmland	Area Not Mapped
Urban Agriculture Incentive Zone	YES
Very High Fire Hazard Severity Zone	No
Fire District No. 1	No
Flood Zone	Outside Flood Zone
Watercourse	No
Hazardous Waste / Border Zone Properties	2000 ft. Buffer Zone for BZP Site (420 Henry Ford Avenue)
Methane Hazard Site	Methane Zone
High Wind Velocity Areas	No
Special Grading Area (BOE Basic Grid Map A-13372)	No
Wells	None
Seismic Hazards	
Active Fault Near-Source Zone	
Nearest Fault (Distance in km)	4.6500288
Nearest Fault (Name)	Newport - Inglewood Fault Zone (Onshore)
Region	Transverse Ranges and Los Angeles Basin
Fault Type	B
Slip Rate (mm/year)	1.00000000
Slip Geometry	Right Lateral - Strike Slip
Slip Type	Poorly Constrained
Down Dip Width (km)	13.00000000
Rupture Top	0.00000000
Rupture Bottom	13.00000000

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 (*) - APN Area is provided "as is" from the Los Angeles County's Public Works, Flood Control, Benefit Assessment.

Dip Angle (degrees)	90.00000000
Maximum Magnitude	7.10000000
Alquist-Priolo Fault Zone	No
Landslide	No
Liquefaction	Yes
Preliminary Fault Rupture Study Area	No
Tsunami Inundation Zone	Yes
Economic Development Areas	
Business Improvement District	None
Hubzone	Not Qualified
Opportunity Zone	Yes
Promise Zone	None
State Enterprise Zone	HARBOR GATEWAY STATE ENTERPRISE ZONE
Housing	
Direct all Inquiries to	Housing+Community Investment Department
Telephone	(866) 557-7368
Website	http://hcidla.lacity.org
Rent Stabilization Ordinance (RSO)	No [APN: 7428014906]
Ellis Act Property	No
AB 1482: Tenant Protection Act	No
Public Safety	
Police Information	
Bureau	South
Division / Station	Harbor
Reporting District	529
Fire Information	
Bureau	South
Batallion	6
District / Fire Station	38
Red Flag Restricted Parking	No

CASE SUMMARIES

Note: Information for case summaries is retrieved from the Planning Department's Plan Case Tracking System (PCTS) database.

Case Number:	CPC-2018-6402-CPU
Required Action(s):	CPU-COMMUNITY PLAN UPDATE
Project Descriptions(s):	ADOPTION OF COMMUNITY PLAN POLICY DOCUMENT; GENERAL PLAN AMENDMENTS, AND ZONE CHANGES TO APPLY RE:CODELA ZONING.
Case Number:	CPC-2010-1238-CRA
Required Action(s):	CRA-COMMUNITY REDEVELOPMENT AGENCY
Project Descriptions(s):	PRELIMINARY PLAN FOR A PROPOSED AMENDMENT TO THE LOS ANGELES HARBOR INDUSTRIAL CENTER REDEVELOPMENT PLAN.
Case Number:	CPC-2005-8252-CA
Required Action(s):	CA-CODE AMENDMENT
Project Descriptions(s):	AN ORDINANCE ESTABLISHING PERMANENT REGULATIONS IMPLEMENTING THE MELLO ACT IN THE COASTAL ZONE.
Case Number:	CPC-1986-833-GPC
Required Action(s):	GPC-GENERAL PLAN/ZONING CONSISTENCY (AB283)
Project Descriptions(s):	AB-283 PROGRAM - GENERAL PLAN/ZONE CONSISTENCY - WILMINGTON-HARBOR CITY-COMMUNITY WIDE ZONE AND GENERAL PLAN CHANGES TO BRINGTHE PLAN AND ZONING INTO CONSISTENCY. INCLUDES CHANGES OF HEIGHT AS NEEDED. REQUIRED BY COURT AS PART OF SETTLEMENT IN HILLSIDE FEDERATION LAWSUIT (TAYLOR-PARKER)
Case Number:	ENV-2019-3379-EIR
Required Action(s):	EIR-ENVIRONMENTAL IMPACT REPORT
Project Descriptions(s):	ENVIRONMENTAL IMPACT REPORT FOR HARBOR GATEWAY AND WILMINGTON-HARBOR CITY COMMUNITY PLAN UPDATE
Case Number:	ENV-2017-2502-CE
Required Action(s):	CE-CATEGORICAL EXEMPTION
Project Descriptions(s):	ENVIRONMENTAL CLEARANCE
Case Number:	ENV-2005-8253-ND
Required Action(s):	ND-NEGATIVE DECLARATION
Project Descriptions(s):	AN ORDINANCE ESTABLISHING PERMANENT REGULATIONS IMPLEMENTING THE MELLO ACT IN THE COASTAL ZONE.

DATA NOT AVAILABLE

ORD_162741
ORD-85015-OD6
ORD-167242
ORD-166001
ORD-164960
ORD-163420

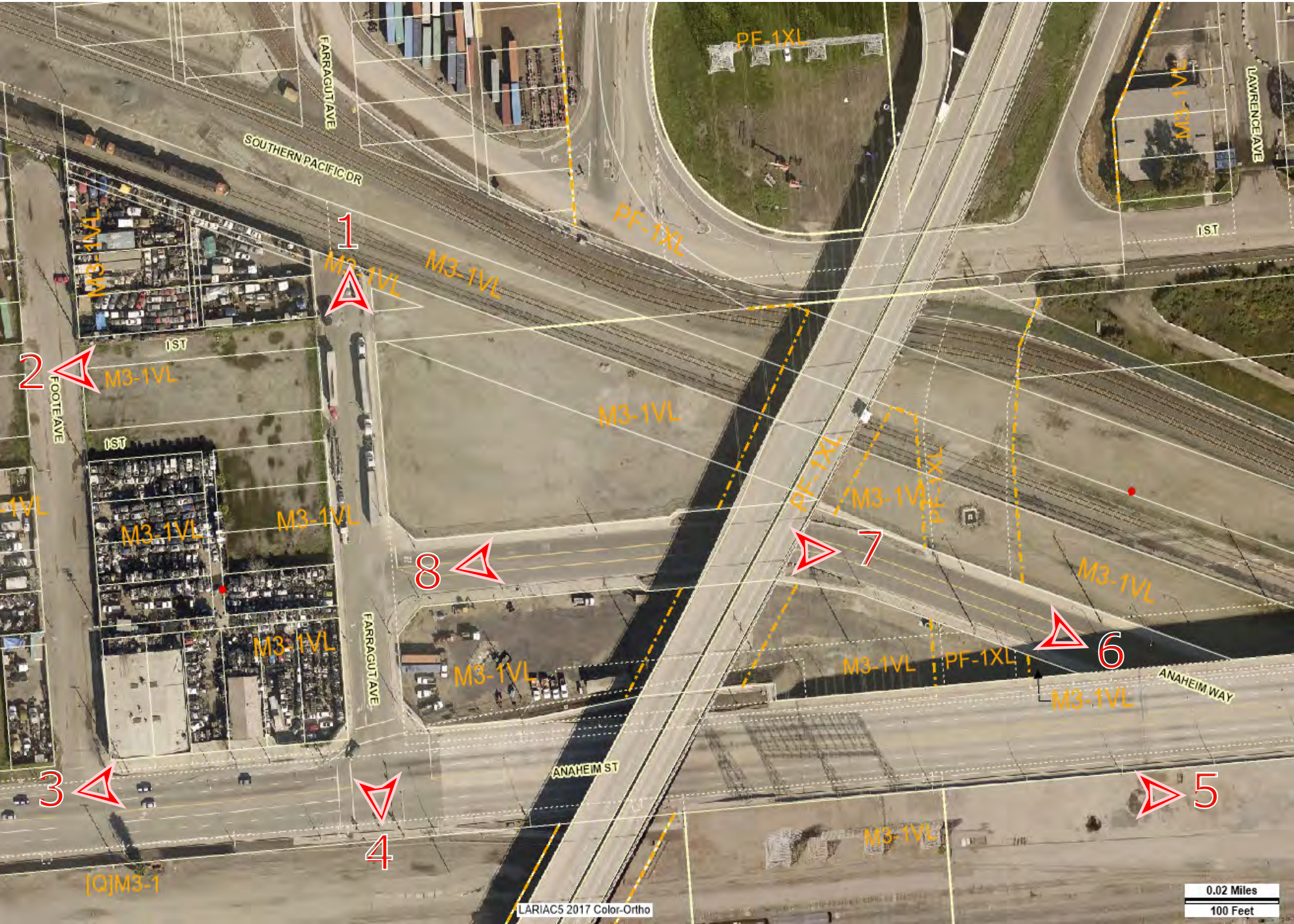


Address: undefined
APN: 7428014BRK
PIN #: 033B217 497

Tract: DOMINGUEZ HARBOR TRACT Zoning: M3-1VL
Block: None
Lot: LOT E
Arb: None
General Plan: Heavy Manufacturing



PHOTO EXHIBIT: GPA FARRAGUT AVE



GPA FARRAGUT AVE

1



GPA FARRAGUT AVE

2



GPA FARRAGUT AVE

3



GPA FARRAGUT AVE

4



GPA FARRAGUT AVE

5



GPA FARRAGUT AVE

6



GPA FARRAGUT AVE

7



GPA FARRAGUT AVE

8



EXHIBIT C

AGENCY CORRESPONDENCE

C1 – Los Angeles Fire Department

C2 – Urban Forestry

CITY OF LOS ANGELES
INTER-DEPARTMENTAL CORRESPONDENCE

April 26, 2022

TO: Vincent Bertoni, AICP, Director of Planning
Department of City Planning
Attention: Connie Chauv

FROM: Los Angeles Fire Department

SUBJECT: **CPC-2020-7285-GPA.:2723 e Anaheim/817-829 Farragut**

Submit plot plans for Fire Department approval and review prior to recordation of City Planning Case.

RECOMMENDATIONS:

Access for Fire Department apparatus and personnel to and into all structures shall be required.

Address identification. New and existing buildings shall have approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property.

One or more Knox Boxes will be required to be installed for LAFD access to project.
Location and number to be determined by LAFD Field Inspector. (Refer to FPB Req # 75).

The entrance or exit of all ground dwelling units shall not be more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.

Fire Lane Requirements:

- 1) Fire lane width shall not be less than 20 feet. When a fire lane must accommodate the operation of Fire Department aerial ladder apparatus or where fire hydrants are installed, those portions shall not be less than 28 feet in width.
- 2) The width of private roadways for general access use and fire lanes shall not be less than 20 feet, and the fire lane must be clear to the sky.
- 3) Fire lanes, where required and dead ending streets shall terminate in a cul-de-sac or other approved turning area. No dead ending street or fire lane shall be greater than 700 feet in length or secondary access shall be required.
- 4) Submit plot plans indicating access road and turning area for Fire Department approval.
- 5) All parking restrictions for fire lanes shall be posted and/or painted prior to any Temporary Certificate of Occupancy being issued.
- 6) Plans showing areas to be posted and/or painted, "FIRE LANE NO PARKING" shall be submitted and approved by the Fire Department prior to building permit application sign-off.
- 7) Electric Gates approved by the Fire Department shall be tested by the Fire Department prior to Building and Safety granting a Certificate of Occupancy.
- 8) All public street and fire lane cul-de-sacs shall have the curbs painted red and/or be posted "No Parking at Any Time" prior to the issuance of a Certificate of Occupancy or Temporary Certificate of Occupancy for any structures adjacent to the cul-de-sac.
- 9) No framing shall be allowed until the roadway is installed to the satisfaction of the Fire Department.

Construction of public or private roadway in the proposed development shall not exceed 10 percent in grade.

The Fire Department may require additional vehicular access where buildings exceed 28 feet in height.

No building or portion of a building shall be constructed more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.

Site plans shall include all overhead utility lines adjacent to the site.

Where access for a given development requires accommodation of Fire Department apparatus, overhead clearance shall not be less than 14 feet.

Private development shall conform to the standard street dimensions shown on Department of Public Works Standard Plan S-470-0.

Standard cut-corners will be used on all turns.

The plot plans shall be approved by the Fire Department showing fire hydrants and access for each phase of the project prior to the recording of the final map for that phase. Each phase shall comply independently with code requirements.

Adequate off-site public and on-site private fire hydrants may be required. Their number and location to be determined after the Fire Department's review of the plot plan.

Any required fire hydrants to be installed shall be fully operational and accepted by the Fire Department prior to any building construction.

All public fire hydrants within the proposed Street Improvement must be relocated at the Petitioners expense with the approval of Department of Water and Power.

The applicant is further advised that all subsequent contact regarding these conditions must be with the Hydrant and Access Unit. This would include clarification, verification of condition compliance and plans or building permit applications, etc., and shall be accomplished **BY APPOINTMENT ONLY**, in order to assure that you receive service with a minimum amount of waiting please call **(213) 482-6543**. You should advise any consultant representing you of this requirement as well.

Kristin M. Crowley
Fire Chief

Orin Saunders, Fire Marshal
Bureau of Fire Prevention and Public Safety

Connie.chauv@lacity.org

April 26, 2022

CPC-2020-7285-GPA.:2723 e Anaheim/817-829 Farragut

Page 3


OS:MRC:mrc

CPC-2020-7285-GPA.:2723 e Anaheim/817-829 Farragut

CITY OF LOS ANGELES
INTER-DEPARTMENTAL CORRESPONDENCE

DATE: May 5, 2022

TO: Vincent Bertoni, Director of Planning
Department of City Planning

FROM: 
Bryan Ramirez, Street Tree Superintendent I
Bureau of Street Services, Urban Forestry Division

SUBJECT: CPC-2020-7285-GPA – 2723 E. ANAHEIM ST.

In regard to your request for review of this case regarding Urban Forestry requirements, it is our recommendation that:

1. STREET TREES

- a. Project shall preserve all healthy mature street trees whenever possible. All feasible alternatives in project design should be considered and implemented to retain healthy mature street trees. A permit is required for the removal of any street tree and shall be replaced 2:1 as approved by the Board of Public Works and Urban Forestry Division.
- b. Plant street trees at all feasible planting locations within dedicated streets as directed and required by the Bureau of Street Services, Urban Forestry Division. All tree plantings shall be installed to current tree planting standards when the City has previously been paid for tree plantings. The subdivider or contractor shall notify the Urban Forestry Division at: (213) 847-3077 upon completion of construction for tree planting direction and instructions.

Note: Removal of street trees requires approval from the Board of Public Works. All projects must have environmental (CEQA) documents that appropriately address any removal and replacement of street trees. Contact Urban Forestry Division at: (213) 847-3077 for tree removal permit information.

BR:djm

EXHIBIT D

ENVIRONMENTAL CLEARANCE

POLB EIR SCH No. 2009081079

D1 – CEQA 15162 Technical Memorandum

D2 – Mitigation Monitoring and Reporting Program

Technical Memorandum

Finding of the Anaheim Way Heavy Haul Project Coverage Within a Prior EIR

Prepared for:



June 23, 2022

Technical Memorandum

Finding of the Anaheim Way Heavy Haul Project
Coverage Within a Prior EIR

Prepared For:

Port of Long Beach
415 W. Ocean Blvd.
Long Beach, CA 90802
Contact: Alex Holford

Prepared By:

Ascent Environmental
15642 Sand Canyon, #54491
Irvine, CA 92619
Contact: Chad Beckstrom, AICP

June 23, 2022

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1 INTRODUCTION

1.1 INTRODUCTION AND OVERVIEW

As part of the Port of Long Beach (POLB or Port) Pier B On-Dock Rail Support Facility (Pier B) Project, POLB is proposing to reconstruct the intersection of Anaheim Way and Farragut Street to widen and realign portions of the roadways to accommodate turning movements of oversized trucks along Anaheim Way from Pier B Street to Farragut Avenue as part of a proposed new Heavy Haul Route. The City of Los Angeles (COLA) requires a general plan amendment to reclassify Anaheim Way and Farragut Street from local streets to collector streets and include both streets in the City of Los Angeles Overweight Vehicle Special Permit Routes, which is a discretionary project pursuant to the California Environmental Quality Act (CEQA).

The proposed improvements to the Anaheim Way and Farragut Street intersection have been previously identified and evaluated within the Final Environmental Impact Report (EIR) for the Pier B Project, certified by the Long Beach Board of Harbor Commissioners (Board) in January 2018¹ (State Clearinghouse# 2009081079). COLA is a responsible agency under CEQA and may use the certified EIR along with any subsequent CEQA documentation to make appropriate findings and approve the project. CEQA procedures for responsible agencies are described further below.

As part of COLA's general plan amendment process, COLA requested that POLB prepare a CEQA analysis to demonstrate that the proposed project was already addressed and is, therefore, within the scope of the certified EIR. This technical memorandum has been prepared to evaluate potential environmental effects associated with the proposed Heavy Haul Route. Specifically, this technical memorandum addresses whether there are any new significant environmental impacts that were not addressed in Pier B EIR, or whether there would be an increase in the severity of any significant impacts addressed in the EIR.

As stated in CEQA Guidelines Section 15162(c):

Once a project has been approved, the lead agency's role in project approval is completed, unless further discretionary approval on that project is required. Information appearing after an approval does not require reopening of that approval. If after the project is approved, any of the conditions described in subdivision (a) occurs, a subsequent EIR or negative declaration shall only be prepared by the public agency which grants the next discretionary approval for the project, if any.

CEQA Guidelines Sections 15162 through 15164, which sets forth criteria to be used to determine appropriate CEQA compliance when considering a project after an EIR has been certified. The analysis within this Technical Memorandum confirms that the environmental effects of the project were covered in the previous EIR with no new significant environmental effects nor any substantially more severe significant effects. Additionally, the analysis identifies mitigation measures that were adopted that are applicable and will be implemented as part of the proposed roadway improvements.

1.2 BACKGROUND OF PIER B ON-DOCK RAIL SUPPORT FACILITY

The Pier B Rail Yard is an important component of overall goods movement handling within the POLB because it is the only rail-serving facility within the Port Complex that can assist the on-dock terminals with the task of assembling trains and dispatching them onto the Alameda Corridor and then, subsequently, to the Class I railroad main lines. The purposes of the Pier B On-Dock Rail Support Facility are to: (a) provide a sufficient facility to accommodate the

¹ Port of Long Beach, 2018. Final Pier B On-Dock Rail Support Facility Project Environmental Impact Report and Application Summary Report. January. Long Beach, CA

expected demand of cargo to be moved via on-dock rail into the foreseeable future; (b) maximize on-dock intermodal operations to reach the long-term goal of 30 to 35 percent of cargo containers to be handled by on-dock rail; (c) provide a facility that can accept and handle longer container trains; and (d) provide a rail yard that is cost effective and fiscally prudent. The Pier B Project would respond to three areas of need: (a) more efficient and rational rail operations, both within and to/from the San Pedro Bay Ports complex; (b) address the physical deficiencies and shortcomings of the existing Pier B Rail Yard with respect to supporting on-dock rail operations; and (c) address local roadway deficiencies and enhance utilities and aging infrastructure.

To maximize the use of on-dock rail, the following are the objectives of the Pier B Project: Support the transition to a more efficient, more economically competitive and less polluting freight transport system as envisioned in the 2016 California Sustainable Freight Action Plan; support the shared goals of local and regional transportation agencies to increase Port, rail and highway capacities; promote a mode shift, from containers shipped by truck to near-dock and/or off-dock facilities to containers shipped by rail from the on-dock and supporting rail yards; provide additional Port rail capability to support and maximize on-dock intermodal operations to a targeted goal of 30 to 35 percent of containers handled by on-dock rail; receive and depart, within the confines of the rail yard, up to 10,000-foot-long trains.

The Pier B Project includes reconfiguring, expanding, and enhancing the capacity of the existing Pier B Rail Yard Facility. The Project will provide a marshaling area to receive and manage the intermodal rail volume growth, provide a destination for westbound trains that currently are not able to enter the port when on dock track space is unavailable, and allow multiple marine terminals to send small cuts of rail cars to be assembled into destination trains.

The EIR identified and analyzed four alternatives offering different configurations and levels of expansion, including a 12th Street Alternative, 10th Street Alternative, 9th Street Alternative, and the No Project Alternative. The 12th Street Alternative was selected by the Port as the Proposed Project and is therefore synonymous and used interchangeably with the Pier B Project. The Pier B Project was proposed to be constructed in three phases over an estimated 7 years. Components of the proposed Project include:

- ▶ Adding 31 yard tracks and five arrival/departure tracks, thereby expanding the yard from an existing 12 tracks (2 main line tracks, 10 yard tracks, and no arrival/departure tracks) to a total of 48 tracks (2 main tracks, 41 yard tracks, and 5 arrival/departure tracks);
- ▶ Providing for up to 10,000-foot long receiving/departure tracks;
- ▶ Providing storage tracks for empty rail cars required to support on-dock intermodal operations and an assembly area for departing trains;
- ▶ Providing staging tracks for non-intermodal cars bound to and from non-container terminals;
- ▶ Widening the existing rail bridge over Dominguez Channel to accommodate one additional track;
- ▶ Constructing an area for locomotive refueling within the yard using tanker truck locomotive refueling vehicles, loaded with fuel offsite; and
- ▶ Realigning and closing some roadways, including closure of the existing at-grade 9th Street railroad grade crossing and removal of the Shoemaker ramps.

1.3 PREVIOUS ENVIRONMENTAL DOCUMENTATION

The City of Long Beach (COLB), acting by and through its Board, prepared the EIR for the Pier B Project to identify and evaluate potential environmental impacts associated with implementation of the Project. POLB, as the public agency project proponent, was the lead agency for compliance with CEQA. A Draft EIR was published on December 16, 2016, for a 90-day public review period which ended on March 13, 2017. POLB prepared a Final EIR which incorporated the Draft EIR as well as responses to comments and minor modifications to the Draft EIR. On January 22, 2018, POLB adopted a resolution to certify the Final EIR for the Pier B Project and approve the project. Mitigation

measures were developed for the project to reduce significant impacts to the extent feasible. These measures were made conditions of project approval and are set forth in the Mitigation Monitoring and Reporting Program adopted as part of the Resolution. POLB made Findings that there are specific overriding economic, legal, technological, and other benefits of the proposed Project that outweigh the significant impacts and provide important reasons for approving the project as proposed; and a Statement of Overriding Considerations was adopted as part of the Resolution. The Final EIR is hereby incorporated by reference.

1.4 CEQA AUTHORITY AND RESPONSIBLE AGENCY REQUIREMENTS

CEQA (Public Resources Code [PRC] Section 21000 et seq.) and its implementing guidelines (CEQA Guidelines; 14 California Code of Regulations [CCR] Section 15000 et seq.) require that all state and local government agencies consider the environmental consequences of projects over which they have discretionary authority prior to taking action on those projects. COLA is a responsible agency pursuant to provisions of the PRC and CEQA Guidelines. Table 1.10-1 on pages 1-56 and 1-57 of the Draft EIR describe the intended uses of the EIR and potential uses of the EIR by other agencies. The potential use by COLA City Council and Municipal Departments is described as follows:

This agency provides permitting authority for building permits within its jurisdiction. The City Council would need to consider whether to undertake property acquisitions to carry out the proposed Project. The Los Angeles City Planning Department will review construction projects located in the coastal zone and gives final authorization for building permit issuance once State and City coastal requirements are established. The City of Los Angeles Department of Transportation provides approval for street vacations, realignments, or additions. The Department of Building and Safety is the COLA's permitting authority for building permits.

The following provisions are relevant to COLA's obligations as a responsible agency under CEQA:

- ▶ **PRC § 21002.1(d). Use of Environmental Impact Reports; Policy.** In applying the policies of subdivisions (b) and (c) to individual projects, the responsibility of the lead agency shall differ from that of a responsible agency. The lead agency shall be responsible for considering the effects, both individual and collective, of all activities involved in a project. A responsible agency shall be responsible for considering only the effects of those activities involved in a project which it is required by law to carry out or approve. This subdivision applies only to decisions by a public agency to carry out or approve a project and does not otherwise affect the scope of the comments that the public agency may wish to make pursuant to Section 21104 or 21153.
- ▶ **PRC § 21069. Responsible Agency [definition].** "Responsible agency" means a public agency, other than the lead agency, which has responsibility for carrying out or approving a project.
- ▶ **CEQA Guidelines § 15050(b). Lead Agency Concept.** Except as provided in subdivision (c), the decision-making body of each Responsible Agency shall consider the Lead Agency's EIR or Negative Declaration prior to acting upon or approving the project. Each Responsible Agency shall certify that its decision-making body reviewed and considered the information contained in the EIR or Negative Declaration on the project.
- ▶ **CEQA Guidelines § 15096. Process For a Responsible Agency**
 - (a) General. A Responsible Agency complies with CEQA by considering the EIR or Negative Declaration prepared by the Lead Agency and by reaching its own conclusions on whether and how to approve the project involved. This section identifies the special duties a public agency will have when acting as a Responsible Agency.
 - (f) Consider the EIR or Negative Declaration. Prior to reaching a decision on the project, the Responsible Agency must consider the environmental effects of the project as shown in the EIR or Negative Declaration. A subsequent or supplemental EIR can be prepared only as provided in Sections 15162 or 15163.
 - (g) Adoption of Alternatives or Mitigation Measures.

- (l) When considering alternatives and mitigation measures, a Responsible Agency is more limited than a Lead Agency. A Responsible Agency has responsibility for mitigating or avoiding only the direct or indirect environmental effects of those parts of the project which it decides to carry out, finance, or approve.
- (h) Findings. The Responsible Agency shall make the findings required by Section 15091 for each significant effect of the project and shall make the findings in Section 15093 if necessary.

1.5 PURPOSE OF THIS MEMORANDUM

To document whether individual projects are within the scope of a previously certified project EIR prepared in accordance with CEQA Guidelines Section 15080 through 15097, and that no new significant impacts would result, the application is reviewed in accordance with Sections 15162 and 15164 of the State CEQA Guidelines. When necessary, additional environmental analysis is completed consistent with Section 15162, including EIR addendums or subsequent EIRs. Pursuant to Section 15162 of the State CEQA Guidelines, no subsequent EIR may be required for a project unless the City determines, on the basis of substantial evidence, that one or more of the following conditions are met:

- (a) When an EIR has been certified or a negative declaration adopted for a project, no subsequent EIR shall be prepared for that project unless the lead agency determines, on the basis of substantial evidence in the light of the whole record, one or more of the following:
 - (1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
 - (2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
 - (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
 - (A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;
 - (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
 - (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
 - (D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

As discussed, a project-level EIR was previously prepared and certified for the Pier B Project, therefore CEQA Guidelines Section 15168 (Program EIR) is not applicable.

2 PROJECT DESCRIPTION

2.1 PROPOSED INTERSECTION AND ROADWAY IMPROVEMENTS

POLB proposes to reconstruct the intersection of Anaheim Way and Farragut Street to widen and realign portions of the roadways to accommodate turning movements of oversized trucks along Anaheim Way from Pier B Street to Farragut Avenue. These improvements would make room for the Port to implement a new Heavy Haul Route, which would replace the 9th Street at-grade crossing route that is scheduled to be permanently closed under the Pier B On-Dock Rail Facility Program. The improvements will enable oversized trucks (approximately 50 annually) to use this route with police escort and the overweight truck route permit from COLA.

The vacant property north of Anaheim Way will be used for the new alignment of the Anaheim Way, and the vacant property east of Farragut Avenue will be used to widen the street (both properties are owned by POLB). Anaheim Way would be widened from the existing 45' to 72' at the intersection with Farragut Avenue. Farragut Avenue would be widened from 44' to 72' just south of the intersection with Anaheim Way. New curbs and sidewalks will be constructed along the new roadways and existing utilities, streetlights, and a catch basin will be reconstructed as a result. A traffic signal pole, streetlight, and their respective infrastructure on the northeast corner of Farragut Avenue/Anaheim Street will also be replaced. The Commercial/Industrial Local Street from COLA's design standard (the Brown Book) will be used for the route design. The route will be striped in a way to match the existing roadway widths and lane assignments, with edge lines and hatched pavement markings to restrict regular traffic from using the excessive pavement/curb-curb width. **Figure 2-1** shows the proposed conceptual improvement plan for the intersection and roadways.

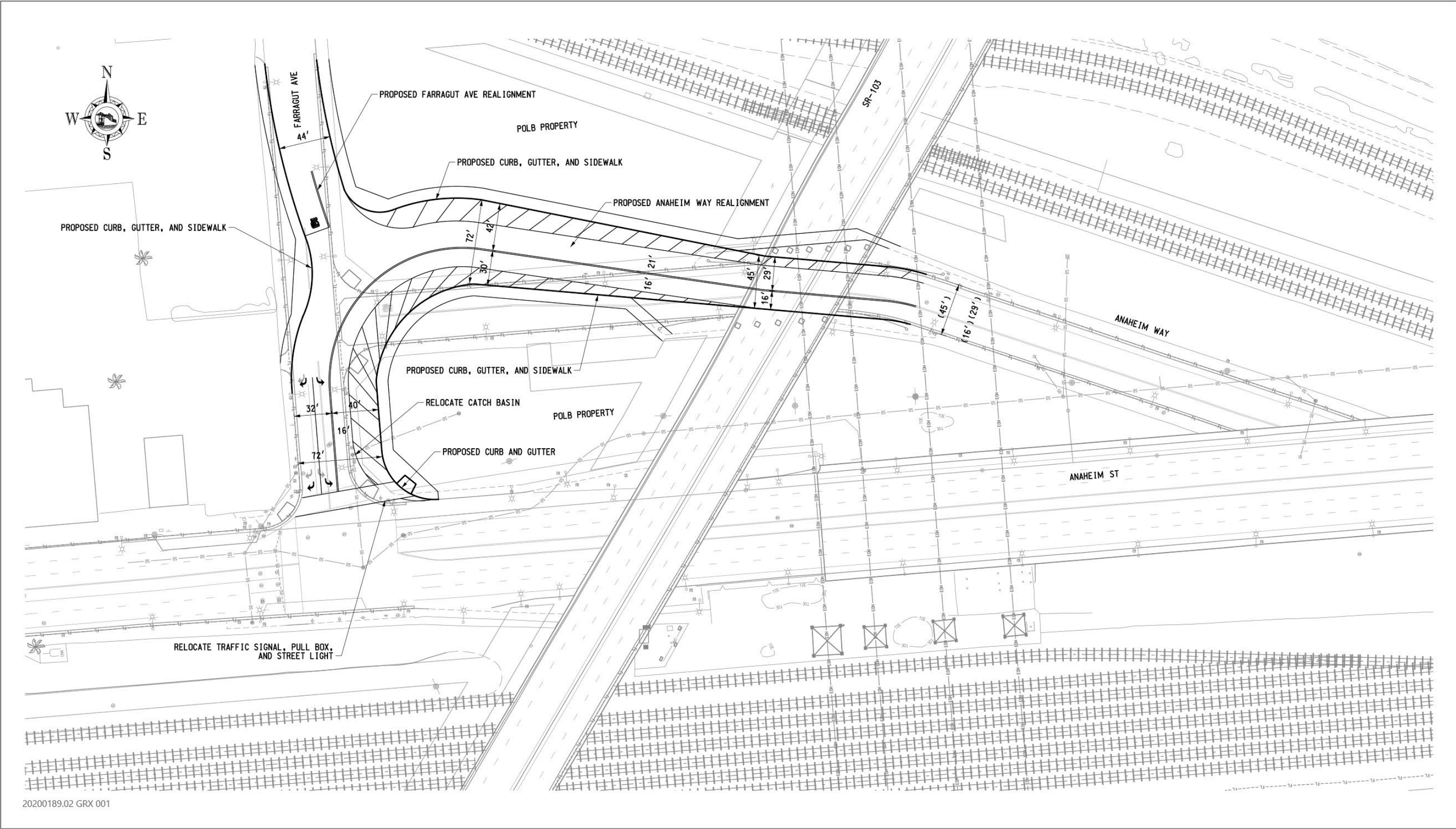
2.2 GENERAL PLAN AMENDMENT AND RECLASSIFICATION

As part of the proposed improvements and to implement the new Heavy Haul Route, COLA is proposing a General Plan Amendment to reclassify Anaheim Way and Farragut Street from local streets to collector streets and to include both streets in the City of Los Angeles Overweight Vehicle Special Permit Routes. No changes are proposed to the existing General Plan land use designation of Heavy Manufacturing or the Zoning designation of M3-1VL for the affected properties needed to expand the intersection.

2.3 CONSISTENCY WITH PIER B EIR PROJECT DESCRIPTION

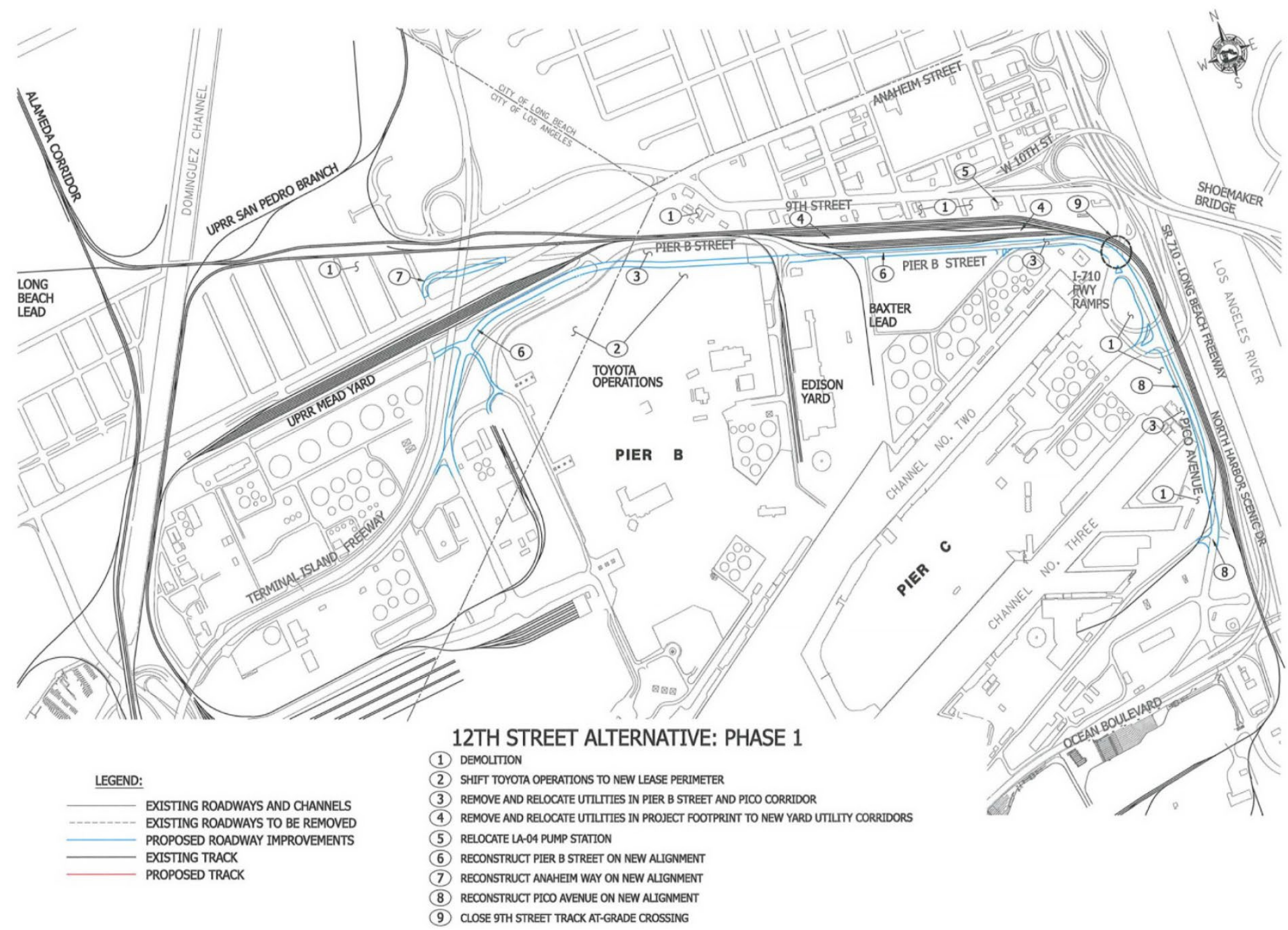
As described in the Pier B EIR, extensive road work would be needed to accommodate the railyard expansion. Specifically, accompanying the description of the "Reconstruction of Anaheim Way on New Alignment" on Page 1-37, which describes realignment of Anaheim Way to make room for proposed construction of rail lines in Phase 2, Figure 1.8-6 on page 1-34 denotes the "Reconstruction of Anaheim Way on New Alignment" (with the number 7), which also shows the realignment of the Anaheim Way/Farragut Street intersection. This figure is presented here as **Figure 2-2**. Therefore, the proposed intersection and roadway improvements were previously envisioned as part of the Pier B Project and were adequately analyzed in the EIR.

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Source: Image prepared by Michael Baker International in 2019, adapted by Ascent Environmental in 2022.

Figure 2-1 Conceptual Intersection Improvement Plan



20200189.02 GRX 002

Source: Image prepared by Parsons in 2016, adapted by Ascent Environmental in 2022.

Figure 2-2 Phase 1 Components of the Pier B Project

3 IMPACT ANALYSIS

The analysis of environmental effects below addresses the same impacts addressed in the Pier B EIR. The environmental analysis evaluates whether, for each environmental resource topic, there are any changes in the project or the circumstances under which it would be undertaken that would result in new or substantially more severe environmental impacts than considered in the EIR. POLB has defined the column headings in the checklist as follows:

- ▶ **Impact Examined in the Final EIR?** "Yes" is stated where the potential impacts of the Project were examined in the Pier B Final EIR. This document summarizes and cross references the relevant analysis in the Final EIR.
- ▶ **Does the Project Involve New or Substantially More Severe Significant Impacts?** This question is answered with a "yes" or "no," as substantiated by the discussion provided below the table. If the response is "yes," additional CEQA analysis is required.
- ▶ **Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?** This question is answered with a "yes" or "no," as substantiated by the discussion provided below the table. If the response is "yes," additional CEQA analysis is required.
- ▶ **Do Mitigation Measures in the Final EIR Address/Resolve Impacts, Including Impacts that Would Otherwise be New or Substantially More Severe?** This question is answered with a "yes" or "no," as substantiated by the discussion provided below the table. The applicable Pier B Final EIR mitigation measures are summarized and cross referenced.

3.1.1 Aesthetics

ENVIRONMENTAL CHECKLIST

Aesthetics	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Have a substantial adverse effect on a scenic vista?	Yes	No	No	N/A
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	Yes	No	No	N/A
c) In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	Yes	No	No	N/A
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.13 of the Pier B EIR describes the existing visual environment and changes resulting from implementation of the Pier B Project. The certified EIR identified no potentially significant aesthetic impacts, as follows:

- ▶ The project area is highly industrial in character and is not located within a scenic vista or other sensitive view location.
- ▶ The Project site is not located in any scenic vista that can be viewed from a scenic route identified in the COLB General Plan Scenic Routes Element or Caltrans Scenic Highway Program. In addition, there are no designated state scenic highways within POLB or the COLB. The nearest state designated state scenic highway is SR 91 beginning at SR 55 to east of the Anaheim city limit, which is more than 20 miles to the northeast of the proposed Project site. The nearest eligible state scenic highway is a segment of SR 1, located approximately 4 miles to the northwest of the proposed Project site that follows the coastline through Orange County into Los Angeles County and terminates at SR 19 in the City of Long Beach. The proposed Project site is not visible from either of these state scenic highways due to distance and intervening buildings and topography.
- ▶ The proposed Project would not introduce aesthetic or visual elements that would degrade the character or quality of existing views. Project elements that were identified that could produce a permanent change in the visual environment include rail yard enhancements, including increased track, reconfiguration of roadways, removal of the ramps to the Shoemaker Bridge, and demolition and construction of buildings. The majority of the project area is not visible from residential or other sensitive areas outside of the Port, and ground-level views would be obstructed, preventing views of the project area. The proposed development would occur within the visual context of a highly industrial area and would not introduce development that is visually incompatible with, or in contrast to, existing Port industrial uses. Therefore, impacts were determined to be less than significant and mitigation measures were not required.

- ▶ The proposed Project would not introduce a source of daytime glare because additional lighting would incorporate modern, anti-glare technology and sensitive receptors are not within sight distance of the Project site. The proposed Project's impact on day or nighttime views were determined to be less than significant and mitigation measures were not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The intersection of Anaheim Way and Farragut Street is within the Pier B Project footprint and, similar to the Pier B Project, is located in a highly industrial area and not within a scenic vista or other sensitive view location. This intersection is also not located in any scenic vista that can be viewed from a scenic route identified in the COLB or COLA General Plans or Caltrans Scenic Highway Program. As with the Pier B Project, the nearest state designated state scenic highway is SR 91 beginning at SR 55 to east of the Anaheim city limit, which is more than 21 miles to the northeast of the intersection. The nearest eligible state scenic highway is a segment of SR 1, located approximately 5 miles to the northeast of the intersection. Thus, the intersection of Anaheim Way and Farragut Street is not visible from either of these state scenic highways due to distance and intervening buildings and topography.

Reconstruction of the intersection of Anaheim Way and Farragut Street would also not introduce aesthetic or visual elements that would change or degrade the character or quality of existing views. The proposed improvements would also not introduce a new source of substantial light or glare which would adversely affect day or nighttime views in the area. As with the Pier B Project, the intersection is not visible from residential or other sensitive areas outside of the Port, and ground-level views would be obstructed, preventing views of the project area. Reconstruction of the intersection would also occur within the visual context of a highly industrial area with existing nighttime lighting and would not introduce development or lighting that is visually incompatible with, or in contrast to, surrounding industrial uses. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Aesthetics/Visual Resources were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.2 Agricultural and Forestry Resources

ENVIRONMENTAL CHECKLIST

Agricultural and Forestry Resources				
Would the Project...	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	Yes	No	No	N/A
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	Yes	No	No	N/A
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	Yes	No	No	N/A
d) Result in the loss of forest or agricultural land or conversion of forest land to non-forest or non-agricultural use?	Yes	No	No	N/A
e) Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or conversion of forest land to non-forest use?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.0.4 of the Pier B EIR, Environmental Resources Not Affected by the Proposed Project, notes that the scoping process determined that no agricultural resources occur on or near the project site; therefore, there would be no impacts on such resources. Consequently, no further evaluation of the environmental consequences on agricultural resources is provided in this EIR.

IMPACTS OF THE PROPOSED PROJECT

There are no agricultural or forestry resources within or near the project area. Therefore, the proposed Project would not have the potential to impact these resources.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Agricultural & Forestry Resources were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.3 Air Quality

ENVIRONMENTAL CHECKLIST

Air Quality				
Would the Project...	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
a) Conflict with or obstruct implementation of the applicable air quality plan?	Yes	No	No	N/A
b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is in non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	Yes	No	No	Yes
c) Expose sensitive receptors to substantial pollutant concentrations?	Yes	No	No	N/A
d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.2 of the Pier B EIR addresses the potential impacts on air quality and human health that could result from implementation of the Pier B Project. The certified EIR identified significant air quality impacts, as follows:

- ▶ During a peak day of construction activity, unmitigated proposed Project construction would produce emissions of volatile organic compounds (VOC), carbon monoxide (CO), nitrogen oxides (NOX), and particulate matter less than 2.5 microns (PM_{2.5}) that would exceed SCAQMD daily emission significance thresholds. Additionally, unmitigated proposed Project construction would result in offsite ambient air pollutant concentrations that would exceed SCAQMD thresholds of significance for 1-hour State nitrogen dioxide (NO₂), 1-hour federal NO₂, annual NO₂, and annual particulate matter less than 10 microns (PM₁₀). Mitigation Measures AQ-1 through AQ-5 would require emission controls for off-road construction equipment, on-road construction trucks, and fugitive dust. These measures would reduce VOC and PM_{2.5} emissions, and annual PM₁₀ ambient concentrations, to below the significance thresholds. However, with mitigation, construction emissions would still exceed the CO and NOX SCAQMD daily emission thresholds; and ambient concentrations during construction would still exceed the SCAQMD ambient air pollutant thresholds for 1-hour State, 1-hour federal, and annual NO₂. Therefore, these mitigated emissions and ambient concentrations would remain significant and unavoidable.
- ▶ The unmitigated proposed Project would produce peak daily operational emissions of CO and NOX that would exceed the SCAQMD impact significance thresholds. Operational emissions of all other criteria pollutants would be below the significance thresholds. Additionally, unmitigated proposed Project operation would result in offsite ambient air pollutant concentrations that would exceed SCAQMD thresholds of significance for 1-hour federal NO₂ and annual NO₂. The proposed Project already incorporates many regulations and CAAP measures that reduce air pollutant impacts. There are no additional feasible mitigation measures identified for Project operation at present. However, to keep pace with emerging emission reduction technologies, a mandatory 5-year technology review would be made part of the Project as a Special Condition (see Section 6.3.2).

- ▶ Unmitigated proposed Project operational activities would generate air pollutants due to the combustion of diesel fuel with attendant diesel exhaust odor. The mobile nature of most proposed Project emission sources would help to decentralize, disperse, and dilute proposed Project emissions over the relatively large project site. Therefore, the potential is low for the proposed Project to produce objectionable odors. Therefore, impacts would be less than significant, and mitigation would not be required.
- ▶ Unmitigated emissions of toxic air contaminants (TAC) from Project construction and operation in comparison to CEQA baseline emissions would exceed the individual significance criterion of 10 in 1 million cancer risk for residential and sensitive receptors. The individual cancer risk for occupational receptors would be less than significant. The population cancer burden would also exceed the significance threshold of 0.5 additional cancer cases. The chronic and acute non-cancer hazard indices would be less than significant for all receptor types. Mitigation Measures AQ-1 through AQ-5 would reduce the individual cancer risks to less than significant levels at all affected residential and sensitive receptors. The population cancer burden would also be reduced to less than the significance threshold. All other predicted health values would remain less than significant. Therefore, with mitigation, the exposure to TAC associated with construction and operation of the proposed Project would be less than significant.
- ▶ The proposed Project would not conflict with or obstruct implementation of the Air Quality Management Plan (AQMP). The proposed Project would comply with the AQMP emission reduction measures that are designed to bring the South Coast Air Basin (SCAB) into attainment of the State and national ambient air quality standards. Because the AQMP assumes growth associated with the proposed Project, it would not exceed the future growth projections in the AQMP, and it would neither conflict with nor obstruct implementation of the State Implementation Plan (SIP). Therefore, impacts would be less than significant, and mitigation would not be required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Table A1.1-8 (Appendix A1 of the Draft EIR) identifies the Anaheim Way alignment reconstruction as part of the Phase 1 emissions, which were applied to the estimates of construction emissions from vehicles and equipment in subsequent emissions tables. These emissions were quantified and consolidated for presentation in Table 3.2-7 of the Draft EIR (page 3.2-32) and included with the analysis of construction emissions in Section 3.2.3.4, Impact AQ-1 (pages 3.2-31 through 3.2-39). No additional construction sources of emissions are anticipated by the proposed Project that have not already been analyzed.

The project would not generate new vehicle trips or substantially increase VMT. Heavy Haul loads currently access POLB, and this project would not result in changes to the nominal number of loads anticipated annually. Thus, the project would not contribute to operational emissions.

Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

While the Proposed Project itself would not result in new significant impacts that require mitigation, it would contribute to construction-related impacts that were previously disclosed and analyzed. Therefore, the following mitigation measures (included in the EIR) would be applicable and required for the proposed Project:

- ▶ **Mitigation Measure AQ-1:** On-Road Construction Trucks. All on-road heavy-duty trucks with a fifth-wheel tractor/trailer and a gross vehicle weight rating (GVWR) of 19,500 pounds or more transporting materials to and from the construction site shall meet EPA 2010 on-road heavy-duty diesel engine emission standards.

- ▶ **Mitigation Measure AQ-2:** Tier 4 Construction Equipment. All self-propelled, diesel-fueled off-road construction equipment 25 horsepower (hp) or greater shall meet EPA/CARB Tier 4 off-road engine emission standards.
- ▶ **Mitigation Measure AQ-3:** Off-Road Construction Equipment. Off-road diesel-powered construction equipment shall comply with the following:
 - Maintain all construction equipment according to manufacturer's specifications.
 - Construction equipment shall not idle for more than 5 minutes when not in use.
 - High-pressure fuel injectors shall be installed on construction equipment vehicles.
- ▶ **Mitigation Measure AQ-4:** Increased Watering Frequency for Fugitive Dust Control. Construction site watering, which would be required by SCAQMD Rule 403, shall be increased such that the watering interval is no greater than 2.1 hours.
- ▶ **Mitigation Measure AQ-5:** Additional Fugitive Dust Control. Contractors shall:
 - Apply approved nontoxic chemical soil stabilizers according to manufacturers' specifications to all inactive construction areas or replace groundcover in disturbed areas.
 - Provide temporary wind fencing around sites being graded or cleared.
 - Cover truck loads that haul dirt, sand, or gravel or maintain at least 2 feet of freeboard in accordance with Section 23114 of the California Vehicle Code.
 - Install wheel washers where vehicles enter and exit unpaved roads onto paved roads or wash off tires of vehicles and any equipment leaving the construction site.
 - Install wheel washers where vehicles enter and exit unpaved roads onto paved roads or wash off tires of vehicles and any equipment leaving the construction site.
 - Suspend all soil disturbance activities when winds exceed 25 miles per hour (mph) or when visible dust plumes emanate from the site and stabilize all disturbed areas.

3.1.4 Biological Resources

ENVIRONMENTAL CHECKLIST

Biological Resources	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?	Yes	No	No	Yes
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or US Fish and Wildlife Service?	Yes	No	No	N/A
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	Yes	No	No	N/A
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	Yes	No	No	N/A
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	Yes	No	No	N/A
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.4 of the Pier B EIR identifies the existing conditions of biota and habitats within the Port and evaluates potential impacts on these resources from the Pier B Project. The certified EIR identified significant biological resources impacts, as follows:

- Construction and operational activities would not substantially affect any rare, threatened, or endangered species or their habitat; interfere with wildlife movement or migration corridors; result in a substantial loss or alteration of marine habitat; substantially affect a natural habitat or plant community, including wetlands; nor substantially disrupt local biological communities.

- ▶ There is no habitat within the Project site for State or federally listed threatened or endangered species. The proposed Project area is fully developed and does not facilitate movement of wildlife within the Port/Project area for birds or terrestrial wildlife. The Dominguez Channel would be the same as it is now, during construction, and during future operations of the proposed Project. The proposed Project area does not include any marine habitats.
- ▶ The proposed Project area is fully developed. There are no biological communities or natural habitats that occur within the proposed Project area. Species within the proposed Project area are already well adapted to the heavily industrialized conditions of the proposed Project area. Construction and operational activities would not disrupt the existing local biological communities or natural habitats of the proposed Project area.
- ▶ There is a potential for bats to be present at the Dominguez Channel rail bridge and migratory birds to be nesting in landscaping, including ornamental trees that would be removed as part of construction. The loss of migratory birds and bats from Project construction would be a potentially significant impact. To avoid potentially significant impacts to bats and migratory birds that could result from construction activities, the two following mitigation measures would be required: (a) a qualified bat specialist will conduct a pre-construction survey, and appropriate subsequent actions would be identified and implemented; (b) construction activities that could remove trees or structures that may support the nests of protected birds would follow the requirements of the Migratory Bird Treaty Act (MBTA). With incorporation of these two mitigation measures, impacts to bats and migratory birds would be considered less than significant.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The intersection of Anaheim Way and Farragut Street is within the Pier B Project footprint and, similar to the Pier B Project, is located in a fully developed highly industrial area and construction and operational activities would not affect any rare, threatened, or endangered species or their habitat; interfere with wildlife movement or migration corridors; result in a substantial loss or alteration of marine habitat; substantially affect a natural habitat or plant community, including wetlands; nor substantially disrupt local biological communities. There are also no biological communities or natural habitats that occur within the area of the intersection and thus construction and operational activities would not result in significant impacts on existing local biological communities or natural habitats.

The intersection is approximately 1.3 miles west of Dominguez Channel and does not involve any activities in or near the channel. Thus, construction and operational activities would not result in adverse effects to marine habitats nor the removal of or other impacts to structures (bridges) or trees that provide habitat for bats or birds.

Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

While mitigation was identified to minimize impacts to bats and migratory birds, the Proposed Project would not result in the removal of or other impacts to structures (bridges) or trees that provide habitat for bats or birds. Therefore, mitigation is not required for the proposed Project.

3.1.5 Cultural Resources

ENVIRONMENTAL CHECKLIST

Cultural Resources	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Cause a substantial adverse change in the significance of a historical resource as defined in Section 15064.5?	Yes	No	No	N/A
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	Yes	No	No	N/A
c) Disturb any human remains, including those interred outside of formal cemeteries?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.12 of the Pier B EIR provides information on known archaeological and historical resources that exist on the Project site and analyzes the potential impacts on known and unknown cultural resources during construction and operation of the Pier B Project. Additionally, Section 3.12 of the Pier B EIR addressed paleontological resources, which are now addressed below in the Geology and Soils Section (Section 3.1.7). The certified EIR identified cultural resources impacts, as follows:

- ▶ No known archaeological resources are located within or near the Project site. Pre-field survey research included a cultural resources records search at the South Central Coastal Information Center (SCCIC), as well as review of National, State, and local inventories of cultural resources to identify local historical events and personages, development patterns, and interpretations of architectural styles. No archaeological resources were identified as a result of this survey. Project construction, therefore, would not reasonably be expected to disturb, damage, or degrade archaeological resources, and mitigation measures are not required.
- ▶ POLB has undertaken a program of identifying and, where feasible, preserving 1950s transit shed structures (including "smoke houses") that remain, as some were demolished during the advent of containerization in the 1960s. The smoke houses located in the Project area were found to be ineligible for listing in the NRHP and the CRHR. An inventory conducted in 2012 for the proposed Project identified 35 buildings and other structures located within and adjacent to the Project area that were more than 50 years of age. All but one of these structures (the Coca-Cola Building) were determined not to be eligible for either the National Register of Historic Places (NRHP) or the California Register of Historical Resources (CRHR). The Coca-Cola Building was located beyond the northern limit of the proposed Project; therefore, the proposed Project would not have a direct impact on the resource. In addition, impacts associated with daily operation of the proposed Project would not have an indirect effect (e.g., noise or vibration) on this resource because the building is located in an urban industrial environment with a neighboring active rail line. Therefore, the proposed Project would not result in an indirect impact, and mitigation measures are not required.
- ▶ No known cultural or historical resources would be utilized or destroyed during construction or operation of the proposed Project, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The intersection of Anaheim Way and Farragut Street is within the Pier B Project footprint and, similar to the Pier B Project, is located in a fully developed highly industrial area and no known archaeological resources were identified within or near the project site. Therefore, as with the Pier B Project, construction activities would not reasonably be expected to disturb, damage, or degrade archaeological resources, and no mitigation is required. In addition, similar to the Pier B Project, because the potential for damaging unknown prehistoric archaeological resources is remote, damage to or destruction of ethnographic resources considered significant to contemporary Native Americans is also not expected. As with the Pier B Project, the proposed Project would be constructed in accordance with the Pier B EIR Special Condition entitled Discovery of Archaeological Materials or Human Remains.

The Pier B EIR identified one structure, Coca-Cola Building, as “appears eligible for CRHR as an individual property through survey evaluation” under the CRHR 3CS status designation. However, the Coca-Cola Building is located beyond the limits of the proposed Project approximately 0.7 mile east of the project site. Therefore, as with the PEIR B Project, the proposed Project would not have a direct impact on the resource. In addition, impacts associated with daily operation of the proposed Project would not have an indirect effect (e.g., noise or vibration) on this resource because the building is located in an urban industrial environment with a neighboring active rail line. Therefore, the proposed Project would not result in an indirect impact, and no mitigation is required.

Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Cultural Resources were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

However, the EIR included Special Conditions for certain resource areas, including cultural resources, which are identified in Section 6.3.6 (page 6-7). Although the potential for disturbing unknown prehistoric remains is remote, standard procedures would apply if unexpected discoveries occur during construction to address potential discovery of subsurface cultural materials, and include the following:

- ▶ In the unlikely event that any archaeological material is discovered during construction, Permittee shall halt all work within the vicinity of the archaeological discovery until a qualified archaeologist completes an assessment detailing the significance of the find. If the resources are found to be significant, they shall be avoided or mitigated consistent with State Office of Historic Preservation (OHP) Guidelines. Treatment plans must be developed in consultation with the county, OHP, and local Native Americans.
- ▶ If human remains are encountered during earth-moving activities, the Los Angeles County coroner shall be contacted immediately. If the remains appear to be Native American, the coroner shall contact the Native American Heritage Commission (NAHC), which will appoint the Most Likely Descendent. Additionally, if the human remains are determined to be Native American, a plan will be developed regarding the treatment of human remains and associated burial objects. This plan will be implemented under the direction of the Most Likely Descendent.
- ▶ Permittee shall immediately notify the Director of Environmental Planning of any discoveries.

3.1.6 Energy

ENVIRONMENTAL CHECKLIST

Energy	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	Yes	No	No	N/A
b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.11, Utilities, Service Systems, and Energy Conservation, of the Pier B EIR provides an analysis of the Pier B Project's potential impacts on energy resources (electricity, natural gas, and petroleum fuels) prepared in accordance with Appendix F of the CEQA Guidelines. The certified EIR identified energy impacts, as follows:

- ▶ Construction and operation of the proposed Project would be consistent with established energy conservation plans and policies. The improvements proposed would result in greater energy efficiency in the future. Therefore, impacts would be less than significant, and mitigation measures are not required.
- ▶ Energy consumption for proposed Project construction would be approximately 180 billion British thermal units (GBtu) over 8 years, or approximately 23 GBtu per year. Energy consumption for proposed Project construction would be used efficiently and would represent a negligible portion of Statewide energy consumption. Therefore, impacts would be less than significant, and mitigation measures are not required.
- ▶ Operational energy consumption under the proposed Project would employ state-of-the art methods and equipment, and it would support a substantially greater level of train operations at Pier B, making more efficient use of existing facilities. Onsite refueling and brake testing under the proposed Project would be more efficient than accomplishing these activities offsite. The expanded facilities would allow longer rail car cuts, reducing switching locomotive operations and decreasing the time and energy to assemble and disassemble trains. New equipment would be required to meet California energy efficiency standards. Furthermore, moving containers by rail instead of truck drayage operations would offset at least 90 percent of the increase in energy consumption from expanded rail yard operations by the year 2035. Impacts of the proposed Project on energy resources would be less than significant, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Due to the short and temporary nature of construction activities associated with reconfiguration of the intersection and the operational heavy haul traffic associated with rerouting the 9th Street at-grade crossing to the reconfigured intersection, implementation of the proposed project would not result in the unnecessary, inefficient, or wasteful use of energy nor would it conflict with any plan, policy, or regulation adopted for the purpose of avoiding

or mitigating environmental effects related to energy use. As with the Pier B Project, energy consumption for proposed Project construction would be used efficiently and would represent a negligible portion of Statewide energy consumption. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Energy were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.7 Geology and Soils

ENVIRONMENTAL CHECKLIST

Geology and Soils	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	Yes	No	No	N/A
ii) Strong seismic ground shaking?	Yes	No	No	N/A
iii) Seismic-related ground failure, including liquefaction?	Yes	No	No	N/A
iv) Landslides?	Yes	No	No	N/A
b) Result in substantial soil erosion or the loss of topsoil?	Yes	No	No	N/A
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	Yes	No	No	N/A
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	Yes	No	No	N/A
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	Yes	No	No	N/A
f) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	Yes	No	No	Yes

SUMMARY OF FINAL EIR FINDINGS

Section 3.1 of the Pier B EIR provides information on geology, soils, and seismic hazards that exist on the Project site and analyzes the potential impacts related to geology and soils resources during construction and operation of the Pier B Project. Additionally, Section 3.12 of the Pier B EIR provides information on paleontological resources that exist on the Project site and analyzes the potential impacts on paleontological resources during construction and operation of the Pier B Project. The certified EIR identified geology and soils impacts, as follows:

- ▶ Figure 3.1-1 shows the locations of nearby faults, including the Palos Verdes Fault (2.4 miles west of site, capable of M 6.5-7.3 earthquakes). Newport-Inglewood Structural Zone (3.6 miles east-northeast of the site, capable of M 6.5-7.5 earthquakes), and Cabrillo Fault (5 miles southwest of site, capable of M 6.0-6.8 earthquakes).
- ▶ The proposed Project is not located on an active fault; therefore, ground rupture at the site and attendant damage to structures is not anticipated. Because there are no known active or potentially active faults crossing the proposed Project area, impacts would be less than significant, and mitigation measures are not required.
- ▶ Seismic activity along numerous regional faults could produce ground shaking, liquefaction, differential settlement, or other seismically induced ground failure. Construction in accordance with COLB and COLA Building Code requirements would limit the severity of consequences from severe seismically induced ground movement.
- ▶ Impacts from construction and operation of the proposed project on geology, groundwater, soils, and seismic conditions would be less than significant, and mitigation measures are not required.
- ▶ Although no previously recorded fossil locality is recorded from the Project area, several are recorded from its vicinity in areas immediately underlain by older and younger alluvium. The Project area of influence is considered to have a high sensitivity for paleontological resources. Project construction could result in the permanent loss of, or loss of access to, paleontological resource that are unearthed at the site. To avoid or minimize the potential for a significant impact to paleontological resources, two mitigation measure will be implemented: (a) paleontological monitoring program should be implemented during earthmoving with excavation at 5 feet or more below ground surface in areas underlain by younger alluvium, or where such activities encounter younger alluvium below any artificial fill; and (b) temporary halting of construction work in the immediate vicinity of a discovery of potentially fossiliferous materials until a qualified vertebrate paleontologist can evaluate the discovery and implement appropriate treatment measures.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The intersection of Anaheim Way and Farragut Street is within the Pier B Project footprint and, similar to the Pier B Project, is not located on an active fault; therefore, ground rupture at the site and attendant damage is not anticipated. In addition, construction would comply with COLB and COLA Building Code requirements to minimize impacts associated with seismically induced geologic hazards including ground shaking, liquefaction, differential settlement, and other seismically induced ground failure. Due to the fully developed and paved condition as well as the flat topography of the project area construction activities would not result in substantial topsoil or wind erosion. Moreover, and consistent with the Pier B Project, the proposed Project would implement best management practices as required by either the General Construction Activity Stormwater Permit or a site-specific Stormwater Pollution Prevention Plan to minimize the amount of soils runoff and wind erosion. Thus, the proposed Project would not result in substantial soil erosion or the loss of topsoil, or trigger or accelerate such processes; alteration of the topography would not occur beyond that resulting from natural erosion and depositional processes.

As with the Pier B Project, the Project area is considered to have a high sensitivity for paleontological resources and project construction could result in the permanent loss of, or loss of access to, paleontological resource that are unearthed at the site. With implementation of Mitigation Measures CR-1 and CR-2, to avoid and minimize the potential for a significant impact to paleontological resources.

Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures are required to address geologic seismic hazards. While the Proposed Project itself would not result in new significant impacts to paleontological resources that require mitigation, it would contribute to construction-related impacts that were previously disclosed and analyzed. Therefore, the following mitigation measures (included in the EIR) would be applicable and required for the proposed Project:

- ▶ **Mitigation Measure CR-1.** Paleontological Monitoring. Because of the Project area's potential for containing buried paleontological resources including fossilized remains of Pleistocene land mammals beginning at depths of 5 feet below the surface, a paleontological monitoring program should be implemented during earthmoving with excavation at 5 feet or more below ground surface in areas underlain by younger alluvium, or where such activities encounter younger alluvium below any artificial fill.
- ▶ **Mitigation Measure CR-2.** Inadvertent Discovery of Paleontological Resources. In the event that construction activities encounter potentially fossiliferous materials, work in the immediate vicinity will be temporarily halted until a qualified vertebrate paleontologist can evaluate the discovery and implement appropriate treatment measures.

Additionally, the EIR included Special Conditions for certain resource areas, including water resources protection from potential erosion, which are identified in Section 6.3.1 (page 6-4) of the Pier B EIR. A Storm Water Pollution Prevention Plan (SWPPP) would be prepared before, and implemented during, construction activities for all projects undertaken in the Port, which includes the following:

- ▶ Prior to the start of construction, Permittee shall obtain coverage under the Los Angeles Regional Water Quality Control Board's General Permit for Storm Water Discharges Associated with Construction and Land Disturbing Activities (CAS000002). A copy of the Notice of Intent (NOI) and SWPPP shall be provided to the Director of Environmental Planning prior to the start of construction.

3.1.8 Greenhouse Gas Emissions

ENVIRONMENTAL CHECKLIST

Greenhouse Gas Emissions	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	Yes	No	No	Yes
b) Conflict with an applicable plan, policy, or regulation adopted for the purpose or reducing the emissions of greenhouse gases?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.14 of the Pier B EIR describes the types of greenhouse gas (GHG) emissions and regulations that would apply to GHG emitted from the proposed Project, as well as the potential impacts from GHG emissions that would result from construction and operation of the proposed Project. The certified EIR identified GHG impacts, as follows:

- ▶ The proposed Project would produce greenhouse gas (GHG) emissions during construction and operations. Annual carbon dioxide equivalent (CO₂e) emissions operations of the proposed Project would remain higher than the SCAQMD interim significance threshold for industrial projects of 10,000 metric tons (MT) per year of CO₂e in all analysis years and would, therefore, constitute a significant impact. The greatest contributor to GHG emissions in all analysis years would be line haul locomotives.
- ▶ While not quantified in the analysis, implementation of air quality Mitigation Measures AQ-1 and AQ-3 would also reduce GHG emissions during construction of the proposed Project. Additional mitigation measures GCC-1 through GCC-7 would further reduce GHG emissions. However, because the effectiveness of these mitigation measures was not quantified and cannot be determined, the impacts of GHG emissions from the proposed Project would remain significant and unavoidable.
- ▶ The proposed Project would not conflict with applicable climate change-related plans, policies, or regulations. Therefore, this impact is less than significant.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Table A1.1-8 (Appendix A1 of the Draft EIR) identifies the Anaheim Way alignment reconstruction as part of the Phase 1 emissions, which were applied to the estimates of construction GHG emissions as well as GHG amortized over 30 years in subsequent emissions tables. These emissions were quantified and consolidated for presentation in Table 3.14-2 of the Draft EIR (page 3.14-18 and 3.14-19) and included with the analysis of construction emissions in Section 3.14.3.3, Impact GCC-1 (pages 3.14-17 through 3.14-29). No additional construction sources of GHG emissions are anticipated by the proposed Project that have not already been analyzed.

The project would not generate new vehicle trips or substantially increase VMT. Heavy Haul loads currently access POLB, and this project would not result in changes to the nominal number of loads anticipated annually. Thus, the project would not contribute to operational GHG emissions.

Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

While the Proposed Project itself would not result in new significant impacts that require mitigation, it would contribute to construction-related impacts that were previously disclosed and analyzed. Therefore, the following mitigation measures (included in the EIR) would be applicable and required for the proposed Project:

- ▶ **Mitigation Measure AQ-1. On-Road Construction Trucks.** All on-road heavy-duty trucks with a fifth-wheel tractor/trailer and a gross vehicle weight rating (GVWR) of 19,500 pounds or more transporting materials to and from the construction site shall meet EPA 2010 on-road heavy-duty diesel engine emission standards.
- ▶ **Mitigation Measure AQ-3. Off-Road Construction Equipment.** Off-road diesel-powered construction equipment shall comply with the following:
 - Maintain all construction equipment according to manufacturer's specifications.
 - Construction equipment shall not idle for more than 5 minutes when not in use.
 - High-pressure fuel injectors shall be installed on construction equipment vehicles.
- ▶ **Mitigation Measure GCC-2: Recycling of Construction Materials.** Pursuant to the POLB Sustainable Business Practices Administrative Directive, construction debris must be recycled, reused or otherwise diverted from landfills to the maximum extent possible. Recyclable construction waste generated by the Project shall be taken to an accredited recycling center.

Note: Mitigation Measures GCC-1, GCC-3, GCC-4, GCC-5, GCC-6, and GCC-7 do not apply to the Proposed Project because they are related to buildings or operations that are unrelated to the Heavy Haul Route project.

3.1.9 Hazards and Hazardous Materials

ENVIRONMENTAL CHECKLIST

Hazards and Hazardous Materials	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	Yes	No	No	N/A
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	Yes	No	No	N/A
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	Yes	No	No	N/A
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	Yes	No	No	N/A
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	Yes	No	No	N/A
f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	Yes	No	No	N/A
g) Expose people or structures to a significant risk of loss, injury or death involving wildland fires?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.9 of the Pier B EIR analyzes the potential impacts of hazards and hazardous materials, including contaminated soils and groundwater, associated with the Pier B Project. The certified EIR identified hazards and hazardous materials impacts, as follows:

- ▶ Neither construction nor operational activities would adversely affect the public through the routine transport, storage, use, or disposal of hazardous materials. Hazardous substances could potentially be spilled or exposed during Project construction and operations, but implementation of standard BMPs, proper use and storage of hazardous materials and petroleum products, and proper removal of asbestos-containing materials (ACM), lead based paint (LBP), and polychlorinated biphenyls (PCB), in accordance with applicable federal, State, and local regulations, would result in less than significant Project construction and operational impacts on hazards and hazardous materials.

- ▶ The proposed Project would be constructed in accordance with applicable federal, State, and local regulations; standard BMP; and proper use and storage of hazardous materials and petroleum products to address onsite hazards, including the presence of contaminated soils or groundwater, during construction. Therefore, proposed Project construction would not adversely affect the public or environment as a result of being located on a site that is known to contain hazardous materials. Impacts would be less than significant, and mitigation measures are not required.
- ▶ Although the proposed Project would result in less than significant impacts as a result of being located on a site that is known to contain hazardous materials, special conditions would be imposed on the proposed Project, including establishing a safety plan before work is started; conducting soil and groundwater sampling as necessary; conducting Phase II investigations where appropriate; and performing a risk assessment prior to starting work in possible contaminated areas. Special conditions are discussed in Chapter 6 (Section 6.3.6).
- ▶ Project construction and operations would not adversely affect the public or environment through reasonably foreseeable upset or accident conditions involving the release of hazardous materials into the environment. Should there be a release of hazardous materials resulting from a rail-related accident during Project construction or operation, established emergency/hazardous materials response procedures would be immediately mobilized. Impacts would be less than significant, and mitigation measures are not required.
- ▶ Local agency requirements would be incorporated into construction planning, and appropriate response procedures would be established as required by law. Contractors and the railroads would continue to comply with all emergency response and evacuation regulations. The Project would not impair or interfere with emergency response or evacuation plans. Impacts would be less than significant, and mitigation measures are not required.
- ▶ Neither Project construction nor operational activities would result in noncompliance with State guidelines associated with abandoned oil wells. Implementation of standard California Division of Oil, Gas, and Geothermal Resources (DOGGR) measures would reduce adverse health and safety effects to construction and operational personnel and the general public; therefore, effects would be less than significant, and mitigation is not required.
- ▶ Hazardous materials would not be handled within 0.25 mile of an existing or planned school, so there would be no impact, and mitigation measures are not required.
- ▶ Because hazardous materials used onsite would be handled in accordance with federal, State, and local requirements, impacts would be less than significant, and mitigation measures are not required.
- ▶ Project operational activities would not adversely affect the public or environment through reasonably foreseeable upset or accident conditions involving the release of hazardous materials into the environment. Rail activity associated with hazardous materials in marine containers would be substantially concentrated at the Project site, which would employ established safety procedures for the handling of rail cars. In addition, a well-defined program of immediate actions, notifications, and onsite responses would be in place, which would substantially minimize the likelihood of an incident with harmful exposure. Should there be a release of hazardous materials resulting from a rail-related accident during Project operation, however, established emergency/hazardous materials response procedures would be implemented. Therefore, impacts would be less than significant, and mitigation measures are not required.
- ▶ Onsite hazardous materials and soil and groundwater contamination would be properly managed during construction, so impacts during operations would be less than significant, and mitigation measures are not required.
- ▶ The proposed Project would be incorporated into existing emergency response plans; management of emergency response and evacuation systems would continue to be managed. Standard security measures would be implemented during Project operation, and access to JCCC services would not be impeded. Adequate safeguards and appropriate response procedures would be in place during Project operation, so impacts related

to implementation of or interference with an adopted emergency response or evacuation plan would be less than significant, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The intersection of Anaheim Way and Farragut Street is within the Pier B Project footprint and, similar to the Pier B Project, is located in a highly industrial area and would not introduce any new uses to this area of the Pier B Project. The Project would be constructed in accordance with applicable federal, State, and local regulations and requirements.

Construction and operational activities would not adversely affect the public through the routine transport, storage, use, or disposal of hazardous materials. Impacts related to accidental spills, exposure to and handling of hazardous materials would be minimized through the implementation of standard BMPs and proper use and storage of hazardous materials in accordance with applicable federal, State, and local regulations. Consistent with the Pier B EIR, while the Project is expected to result in less than significant impacts related to being in an area that is known to contain hazardous materials, special conditions listed below would be imposed on the proposed Project to minimize potential impacts.

Construction activities would be temporary lasting approximately 6 months and the new Heavy Haul Route would be updated and reclassified as part of the proposed General Plan amendment to the Circulation Element and would not impair or interfere with emergency response or evacuation plans. As with the Pier B Project, the proposed Project would be incorporated into existing emergency response plans and management of emergency response and evacuation systems would continue to be managed.

No known active production, injection or abandoned wells are located within the intersection of Anaheim Way and Farragut Street and thus construction activities would not result in noncompliance with State guidelines associated with abandoned oil wells. Additionally, no schools are located within 0.25 mile of the intersection and no impacts would occur related to handling hazardous materials near an existing or planning school. The Project site is also not located in an area susceptible to wildland fires and thus would not expose people or structures to significant risks involving wildland fires.

Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Hazards and Hazardous Materials were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

However, the EIR included Special Conditions for certain resource areas, including hazardous materials, which are identified in Section 6.3.5 (pages 6-6 and 6-7) of the Pier B EIR. Site-specific investigations to identify and appropriately manage hazardous materials are required for projects undertaken in the Port, and include the following:

- ▶ Pursuant to the Port requirements and prior to conducting the site investigations, Permittee shall provide to the Director of Environmental Planning the proposed site investigations, including but not limited to soil, risk assessment, safety, aerially deposited lead (ADL), groundwater, asbestos-containing materials (ACM), lead, and treated wood waste (TWW), for review and approval. Permittee shall provide all test results to the Director of Environmental Planning as soon as available.
- ▶ A Phase II Site Investigation shall be performed in construction areas where excavation would exceed 5 feet below ground surface (bgs), where groundwater may be encountered, and in areas where underground storage tanks (UST) were removed without closure. The results of the Phase II investigation shall be incorporated into the Safety Plan to protect construction workers against known contamination in construction areas. A Hazardous

Waste Management Plan based on the results of the Phase II investigation shall also be incorporated in the Final Design to ensure proper disposal of contaminated materials and contaminated groundwater found in the construction areas.

- ▶ A risk assessment shall be performed prior to construction to determine how construction activities would affect the water-bearing levels and, as applicable, to determine health risks to construction workers.
- ▶ A Safety Plan shall be required to address any exposure to hazardous materials. The Safety Plan shall include proper personal protective equipment (PPE) work requirements, soil and air space monitoring requirements, documentation and reporting requirements, and action levels.
- ▶ Prior to construction, areas within the proposed Project corridor where soil may be disturbed shall be tested for ADL. If ADL levels meet or exceed the action level set forth by the Hazardous Waste Management Plan for the Project, ADL-contaminated soils would be removed in accordance with federal, State, and local regulations.
- ▶ To minimize cross contamination of the water-bearing zones, construction techniques to minimize the need for dewatering shall be used.
- ▶ Groundwater displaced or extracted by construction activities shall be contained and tested to guide appropriate storage, discharge, or disposal. Laboratory analyses would include petroleum hydrocarbons (full carbon chain range), Title 22 metals, volatile organic compounds (VOC), Semi-volatile organic compounds (SVOC), polycyclic aromatic hydrocarbons (PAH), pesticides, and polychlorinated biphenyls (PCB).
- ▶ If unexpected, potentially contaminated soil or groundwater is discovered during construction, work shall stop in the affected area. Sampling and analysis of the soil or groundwater shall be conducted to determine proper handling and disposal methods.
- ▶ In all buildings subject to demolition a survey to screen for ACM shall be conducted. ACM shall be removed prior to demolition to mitigate ACM hazards.
- ▶ Lead and other heavy metals, such as chromium, may be present within yellow thermoplastic paint markings on the pavement. A Lead Compliance Plan shall be prepared in accordance with California Code of Regulations (CCR) Title 8 Section 1532.1. The Lead Compliance Plan shall be approved by an industrial hygienist certified in comprehensive practice by the American Board of Industrial Hygiene.
- ▶ An environmental monitoring program during construction shall include soil testing to identify and monitor soils affected by petroleum hydrocarbons or other oil-field hazardous constituents, such as metals. The extent of the testing and monitoring shall be based on the final disposition of the excavated soil. Laboratory analyses shall include petroleum hydrocarbons (full carbon chain range), Title 22 metals, VOC, SVOC, PAH, pesticides, and PCB.
- ▶ Railroad ties shall be managed as TWW. Railroad ties designated for reuse shall be managed in accordance with Alternative Management Standards provided in CCR Title 22 Section 67386. Railroad-tie materials designated for disposal shall be considered potentially hazardous TWW and would be managed and disposed in accordance with Title 22 Section 67386.
- ▶ Shallow surface soils within the railroad right-of-way (ROW) may contain arsenic from historic weed control practices and shall be tested for arsenic.

3.1.10 Hydrology and Water Quality

ENVIRONMENTAL CHECKLIST

Hydrology & Water Quality	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?				
b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	Yes	No	No	N/A
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:				
i) result in substantial erosion or siltation on- or off-site;	Yes	No	No	N/A
ii) substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	Yes	No	No	N/A
iii) create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	Yes	No	No	N/A
iv) impede or redirect flood flows?	Yes	No	No	N/A
d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	Yes	No	No	N/A
e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.3 of the Pier B EIR analyzes potential impacts of the Pier B Project on groundwater, surface water, bedded sediments, floodplains, designated beneficial uses, and water quality. Additionally, Section 3.11 of the Pier B EIR provides information and analysis on potential impacts from seiches or tsunamis. The certified EIR identified hydrology and water quality impacts, as follows:

- Construction and operation of the proposed Project would not result in violation of regulatory standards or guidelines. Project construction and operation would not involve any direct or intentional discharges of wastes to harbor waters. All work would be conducted in accordance with Project-specific permits that include measures to minimize impacts to water quality. Leaks or spills of petroleum products from equipment would be handled by

appropriate waste management Construction Site BMP; therefore, the impacts would be less than significant, and mitigation measures are not required.

- ▶ Construction and operation of the proposed Project would not result in exceedances of the Enclosed Bays and Estuaries Plan criteria for sediment-introduced contaminants. Implementation of a Stormwater Pollution Prevention Plan (SWPPP) and Construction Site BMP, and adherence to National Pollutant Discharge Elimination System (NPDES) permit requirements would be required during construction. Because implementation of the proposed Project would result in a reduction of stormwater runoff, Project operations would have little potential to affect harbor water quality in the immediate vicinity of storm drains and other locations where runoff of soils can enter the harbor. As a result, exceedances of the Enclosed Bays and Estuaries Plan criteria would be less than significant, and mitigation measures are not required.
- ▶ Construction and operation of the proposed Project would not result in flooding. Construction of the proposed Project would not increase the potential for flooding onsite because drainage would be controlled. The design of the stormwater drainage system would safely and adequately convey flows to ensure that there would be no adverse effects to the area hydrology or floodplain. There are no levees or dams in the vicinity. Impacts would be less than significant, and mitigation measures are not required.
- ▶ Construction and operation of the proposed Project would not result in wind or water erosion that would cause substantial soil runoff. Runoff from general construction activities would have short-term, localized less than significant impacts on water quality. Construction and operational activities would not accelerate the natural processes of wind and water erosion and will be controlled onsite through implementation of BMP. Because implementation of the proposed Project would reduce stormwater runoff, Project operations would have little potential to affect harbor water quality in the immediate vicinity of storm drains and other locations where runoff of soils can enter the harbor. Thus, impacts would be less than significant, and mitigation is not required.
- ▶ The proposed Project elevation is approximately 10 to 25 feet above mean lower low water (MLLW) and is inland from the shoreline. There would, therefore, be an extremely low risk of coastal flooding due to tsunamis and seiches.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The intersection of Anaheim Way and Farragut Street is within the Pier B Project footprint and, similar to the Pier B Project, due to the fully developed and paved condition of the project site as well as the flat topography would not result in wind or water erosion that would cause substantial soil runoff. All work would be conducted in accordance with Project-specific permits that include measures to minimize impacts to water quality. Leaks or spills from equipment would be handled by appropriate waste management Construction Site BMP. Moreover, and consistent with the Pier B Project, the proposed Project would implement best management practices as required by either the General Construction Activity Stormwater Permit or a site-specific Stormwater Pollution Prevention Plan to minimize the amount of soils runoff and wind erosion. Construction of the project would also not increase the potential for flooding onsite or result in an increase in surface runoff which would exceed the capacity of existing or planning stormwater drainage systems. The project site is located in an area of extremely low risk of coastal flooding due to tsunamis and seiches and would impede or redirect flood flows or result in a release of pollutants due to project inundation. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Hydrology and Water Quality were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

However, the EIR included Special Conditions for certain resource areas, including water resources protection, which are identified in Section 6.3.1 (page 6-4) of the Pier B EIR. A Storm Water Pollution Prevention Plan (SWPPP) would be prepared before, and implemented during, construction activities for all projects undertaken in the Port, which includes the following:

- ▶ Prior to the start of construction, Permittee shall obtain coverage under the Los Angeles Regional Water Quality Control Board's General Permit for Storm Water Discharges Associated with Construction and Land Disturbing Activities (CAS000002). A copy of the Notice of Intent (NOI) and SWPPP shall be provided to the Director of Environmental Planning prior to the start of construction.

3.1.11 Land Use and Planning

ENVIRONMENTAL CHECKLIST

Land Use & Planning	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Physically divide an established community?	Yes	No	No	N/A
b) Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.6 of the Pier B EIR describes the existing and future land uses and zoning in the vicinity of the Project area and evaluates potential land use and zoning impacts of the proposed Project. Section 3.6.4 provides an evaluation of the impacts of potential land acquisitions that would be required if the proposed Project is approved and implemented. The certified EIR identified land use impacts, as follows:

- ▶ Project construction and operational activities would be consistent with the adopted goals, objectives and policies of applicable local, regional and State plans. Land use impacts due to Project construction and operational activities would be less than significant, and mitigation measures are not required.
- ▶ The proposed Project would be consistent with goals and policies contained within the PMP which seeks to increase primary Port use and encourage more effective use of existing land in the Port. The proposed Project would also be consistent with the COLB Mobility Element which calls for increased on-dock rail support. The proposed Project would address these goals by substantially increasing the efficiency of rail service to and from existing on-dock facilities, thereby increasing economic development. Project operations would not introduce uses or activities incompatible with existing and future land uses. The proposed Project would not physically conflict or interfere with operation of the COLB Multi-Service Center currently 1,270 feet from the existing rail yard. The proposed Project is consistent with the COLA General Plan's Wilmington-Harbor City Community Plan goals and objectives because it represents a continuation of existing land uses. Impacts would be less than significant, and mitigation is not required.
- ▶ The proposed Project would not require relocation of any residences; therefore, it would not require replacement housing elsewhere. POLB, COLB, and COLA would be required to follow procedures and legal requirements for relocations of industrial and commercial properties; adequate compensation would be provided for acquisitions. Construction of replacement buildings or structures would not be required because industrial and commercial space is expected to be available in the North Harbor area. Impacts would be less than significant and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on new alignment was identified within Phase 1 construction for the Pier B Project. The proposed Project would not divide an established community as it represents a minor realignment of an existing roadway within the Port and is not near residential uses. The project would be consistent with the PMP goals and

policies and does not require land use or zoning changes. The General Plan Amendment to reclassify the roadways is an administrative change that would not result in environmental impacts.

Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Land Use and Planning were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.12 Mineral Resources

ENVIRONMENTAL CHECKLIST

Mineral Resources	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	Yes	No	No	N/A
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Mineral Resources are addressed in Section 3.1 Geology, Soils, and Seismic Conditions) of the Pier B EIR, which evaluates the potential impact on the availability of mineral resources. The certified EIR identified mineral impacts, as follows:

- ▶ The Project site is underlain by the Wilmington Oil Field, and the Pier B Project would preclude future onsite oil or gas extraction from within Project boundaries; however, petroleum reserves beneath the site could be recovered from remote locations, using directional (e.g., slant) drilling techniques.
- ▶ No known mineral (including petroleum or natural gas) resources would be rendered inaccessible by the proposed Project. All wells would be abandoned during Project construction in accordance with DOGGR requirements. Although construction activities would remove active and inactive oil-producing facilities from the Project site, petroleum reserves beneath the site could continue to be recovered from nearby active facilities during construction. Accordingly, impacts of the proposed Project related to access to mineral resources would be less than significant, and mitigation measures are not required.
- ▶ No known mineral (including petroleum or natural gas) resources would be rendered inaccessible by operation of the proposed Project. Because petroleum reserves beneath the site could continue to be recovered after the proposed Project becomes operational, impacts of the proposed Project related to access to mineral resources would be less than significant, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. While the project area may overlay oil resources, it would not affect any existing extraction wells, facilities, or operations. The small footprint of the project area would not preclude access to mineral resources. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Mineral Resources were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.13 Noise

ENVIRONMENTAL CHECKLIST

Noise	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project result in...				
a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	Yes	No	No	N/A
b) Generation of excessive groundborne vibration or groundborne noise levels?	Yes	No	No	N/A
c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.8 of the Pier B EIR evaluates the baseline noise and vibration environment and the impacts of the Pier B Project on the noise and vibration environment. The certified EIR identified noise impacts, as follows:

- ▶ Predicted construction noise levels at the sensitive receptors would not increase ambient noise by 3 decibels (dB) or greater, nor would this noise exceed the applicable noise limits and restrictions imposed by COLB or COLA. Construction noise from the proposed Project would have a less than significant impact, and mitigation measures are not required.
- ▶ Predicted construction vibration levels would not exceed the Federal Transit Administration (FTA) groundborne vibration damage criteria for non-engineered timber/masonry buildings or reinforced concrete, steel, or masonry buildings. The predicted vibration level from construction equipment would not result in building damage beyond a distance of 26 feet from the source. Annoyance from construction vibration would not be perceived beyond a distance of 73 feet from the source. Construction vibration from the proposed Project would have a less than significant impact, and mitigation measures are not required.
- ▶ The predicted noise levels at the receptor locations attributable to rail yard operations would be at least 10 dB below baseline ambient noise levels; and the proposed Project maximum noise level (Lmax) is not expected to exceed the measured ambient Lmax or the Long Beach Municipal Code (LBMC) limits; therefore, no impact would result.
- ▶ The expected noise levels from railroad operations would be lower than ambient noise levels at all of the sensitive receptors. Because of this, no exceedances of either the COLB or COLA standards would occur. Impacts from rail yard operations would be less than significant, and mitigation is not required.

- ▶ The proposed Project would not result in ambient operational noise levels that exceed LBMC limits for the equivalent sound level (Leq) or Lmax. Therefore, impacts to ambient noise from operation of the proposed Project would be less than significant, and mitigation measures are not required.
- ▶ Due to the distance of proposed Project operational activities to noise-sensitive receptors in the COLA, the COLA normally acceptable noise levels (50 to 75 A-weighted decibel [dBA] Community Noise Equivalent Level [CNEL]) for this land use category are not expected to be exceeded. Therefore, operational noise impacts in the COLA portion of the Project influence area would be less than significant, and mitigation measures are not required.
- ▶ The proposed Project is estimated to result in a less than 1-dB Leq and Day-Night Level (Ldn) increase in noise along the Alameda Corridor; the overall ambient noise level increase is expected to be less than 1 dB. This increase in ambient noise from proposed Project train activity would not exceed FTA severe impact criteria or add 3 dbA or more above baseline ambient conditions. No significant noise impact is expected as a result of this minimal increase in noise level compared to that of the baseline ambient conditions, and mitigation measures are not required.
- ▶ Because no changes in interior noise levels are expected and the interior noise limit is not expected to be exceeded, the proposed Project operational noise level would not exceed the COLB allowable limit of 45 dBA for interior noise. The impact from proposed Project operational noise levels would be less than significant, and mitigation measures are not required.
- ▶ Vibration generated by proposed Project traffic and rail yard operations would not exceed the FTA acceptability limit of 83 VdB (velocity level in decibels). The impact of the proposed Project on vibration would be less than significant, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Noise impacts from Phase 1 of construction was previously analyzed and included the Anaheim Way realignment as part of the construction activities. Classification of the new Heavy Haul Route would add approximately 50 heavy load truck trips on the new alignment annually, which is less than 1 trip per day, and would therefore not result in changes to operational traffic noise. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Noise were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

However, the EIR included Special Conditions for certain resource areas, including noise, which are identified in Section 6.3.4 (page 6-5). In advance of and during construction activities associated with the proposed Project, notification will be provided to those properties and persons located adjacent to construction activities, and includes the following:

- ▶ Permittee shall publish notices in the Press Telegram, and provide notification to adjacent property managers, owners, agencies, and schools in advance of the construction schedule. Once known, Permittee shall provide to the Director of Environmental Planning a list of all entities that will be notified for review and approval.

3.1.14 Population and Housing

ENVIRONMENTAL CHECKLIST

Population and Housing	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	Yes	No	No	N/A
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.10 of the Pier B EIR identifies the baseline conditions of population, including minorities and low-income populations, and housing near the Project site and evaluates potential impacts to these resources as related to the Pier B Project. The certified EIR identified population and housing impacts, as follows:

- ▶ Proposed Project construction and operational activities would not increase population in the Gateway Cities subregion by the established impact significance threshold of 0.5 percent or more. It is likely that most of the required construction workers already reside in the Gateway Cities subregion. The proposed Project would add a maximum of 10 permanent jobs per work shift, which is considered a negligible increase in total employment in the region compared to the baseline. These jobs would likely be filled by existing residents in the area; therefore, impacts on population as a result of Project construction and operations would be less than significant, and mitigation measures are not required.
- ▶ Proposed Project construction and operational activities would not increase the demand for housing units in the Gateway Cities subregion by the impact significance threshold of 0.5 percent or more. The construction labor force already in the region would be sufficient to complete construction of the proposed Project without workers migrating to the region. No significant impact to housing is anticipated from the 10 additional jobs per work shift generated by the proposed Project. It is expected that these jobs would be filled by existing residents within the Gateway Cities region; therefore, no new housing units would be necessary. Impacts would be less than significant, and mitigation measures are not required.
- ▶ The Pier B EIR included an analysis of disproportionate impacts on minorities and low-income populations, which is not addressed in this Addendum.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Population associated with this construction component was previously analyzed and determined that jobs would be fulfilled from the region thereby not resulting in a significant increase in population growth. Operational impacts from the project would not affect employment or population in any way. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Population and Housing were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.15 Public Services

ENVIRONMENTAL CHECKLIST

Public Services	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i) Fire protection?	Yes	No	No	N/A
ii) Police protection?	Yes	No	No	N/A
iii) Schools?	Yes	No	No	N/A
iv) Parks?	Yes	No	No	N/A
v) Other public facilities?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.7 of the Pier B EIR addresses the existing infrastructure and levels of service as well as the potential impacts to public services that would result from construction and operation of the Pier B Project. The certified EIR identified Public Services impacts, as follows:

- ▶ Project construction activities would not burden police, fire, or other security agency staff levels and acceptable service ratios, response times, and other performance objectives would be maintained.
- ▶ Construction activities requiring roadway closures and modifications would be conducted in accordance with the Transportation Management Plan (TMP).
- ▶ Construction of the proposed Project would not degrade law enforcement response times, emergency service levels, and MSC performance objectives. The additional traffic control services required by proposed Project construction activities are not expected to have a substantial impact on police or fire service levels. The standard security measures to be implemented during construction of the proposed Project would minimize the burden on police, fire, and other security agency staff levels. Therefore, construction of the proposed Project would have a less than significant impact on public services/health and safety, and mitigation is not required.
- ▶ Proposed Project operations would not affect first responder response times, emergency service levels, or performance objectives. The local area street system will be designed such that all required emergency access routes would be made available. Because impacts on public services would be less than significant, mitigation measures are not required. Relocation of fire hydrants, water supply trunk lines, and distribution mains in the proposed Project area would be conducted in consultation with the affected public service agencies and would be appropriately managed so that there would be minimal, if any, disruptions to service. No other impacts to

public facilities are anticipated. All public service locations would continue to be accessible. Operation of the proposed Project would have a less than significant impact on public services and safety; mitigation is not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Impacts on public services associated with this construction component were previously analyzed and determined that construction would not degrade service times, staffing ratios, or performance objectives. A TMP would be prepared which would ensure adequate emergency services access. The project would not affect parks, schools, or other public facilities. Operations would maintain the same access routes as identified in the EIR. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Public Services were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.16 Recreation

ENVIRONMENTAL CHECKLIST

Recreation	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	Yes	No	No	N/A
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.0.4 of the Pier B EIR, Environmental Resources Not Affected by the Proposed Project, notes that the scoping process determined that no recreation resources occur on or near the project site; therefore, there would be no impacts on such resources. Consequently, no further evaluation of the environmental consequences on recreation resources is provided in this EIR.

IMPACTS OF THE PROPOSED PROJECT

There are no recreation resources within or near the project area. Therefore, the proposed Project would not have the potential to impact these resources.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Recreation were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.17 Transportation

ENVIRONMENTAL CHECKLIST

TRANSPORTATION	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Conflict with program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	Yes	No	No	N/A
b) Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	No	No	No	N/A
c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	Yes	No	No	N/A
d) Result in inadequate emergency access?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.5 of the Pier B EIR describes the baseline transportation setting and potential impacts of the Pier B Project. The certified EIR identified Transportation impacts, as follows:

- ▶ Construction-related activities are not expected to use rail services, so there would not be a need to increase rail service to accommodate project construction. Therefore, there would be no significant impact on the regional rail network and no delays at regional grade crossings. Some construction traffic may cross the tracks at the following grade crossings in the Port vicinity: Pier B Street/9th Street, Pier B Street/Anaheim Way, Pier B Street/Baker Lead, Pier B Street/Edison Avenue, and Pico Avenue/West Pier D Street. Project construction activities would only have minor impacts on these grade crossings, and mitigation is not required.
- ▶ Construction-period increases in auto and truck traffic would not exceed established level of service (LOS) thresholds at study area intersections. Traffic generated by construction activities would not have short-term significant impacts exceeding volume-to-capacity (V/C) ratio thresholds on highway segments in the study area, including the eastbound and westbound roadway segments on Pacific Coast Highway (PCH) at the overpass of the PCH/I-710 interchange, and eastbound and westbound roadway segments on PCH at the Los Angeles River, and mitigation measures are not required. The proposed Project would not have significant impacts at any intersections within the study area, and mitigation is not required.
- ▶ Construction would not conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities. Pedestrians are not allowed within the Pier B Rail Yard, but they would still have access to all businesses on streets not directly within the proposed Project footprint, including the Multi-Service Center (MSC). The proposed Project would not conflict with any adopted policies, plans, or programs regarding pedestrian facilities, and mitigation is not required.
- ▶ There are no bicycle paths within the proposed Project footprint. The nearest bike lane in the City of Los Angeles (Wilmington neighborhood) runs along Anaheim Street from Western Avenue to North Henry Ford Avenue (SR 47) and is part of the COLA backbone bikeway network. This Anaheim Street bike lane continues from Henry

Ford Avenue to 9th Street/I Street. The nearest bike path in the City of Long Beach runs alongside the eastern side of the Los Angeles River. The Mark Bixby Memorial Bicycle Pedestrian Path, a Class I bikeway (bike path), will be included as part of the new Gerald Desmond Bridge connecting from SR 47 to Pico Avenue. Construction of the proposed Project, primarily railroad track improvements north and south of Anaheim Street, would not affect the bike lane because the existing bike paths along Anaheim Street are elevated above the rail yard.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Construction may result in some construction traffic crossing the tracks, particularly at Pier B Street/Anaheim Way. As identified in the EIR, project construction activities would only have minor impacts on these grade crossings, and mitigation is not required. Construction increases in traffic for construction of the roadway improvements were considered in the EIR and determined that the short-term increases would not result in significant impacts. Additionally, no conflicts with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities were identified.

Operations would not generate new vehicle trips within the Port. However, an additional approximately 50 heavy haul trucks annually could use the reconfigured intersection. The intersection of Farragut Avenue/Anaheim Way was not evaluated in the EIR. However, the nearby intersections of Farragut Avenue/East Anaheim Street and Pier B Street/Anaheim Way were evaluated. As shown in Table 3.5-5 of the Pier B EIR, both intersections were operating at level of service (LOS) A under baseline conditions. As shown in Table 3.5-13 of the Pier B EIR, with the Pier B project under 2035 conditions both intersections would remain at LOS A. The addition of 50 trucks annually to these intersections would not result in changes to operations of these intersections. Reconfiguring the intersection of Farragut Avenue/Anaheim Way would improve the safety of the intersection for heavy haul trucks and other truck and automobile traffic. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Transportation were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

However, the EIR included Special Conditions for certain resource areas, including Transportation, which are identified in Section 6.3.3 (page 6-5). The Port requires a Transportation Management Plan (TMP) to minimize traffic congestion during project construction, and includes the following:

- ▶ Permittee shall prepare a TMP that includes measures to minimize transportation impacts during construction. The TMP shall be prepared in consultation with Port staff and, at a minimum, include the following elements:
 - Public Information Plan
 - Traveler Information
 - Incident Management
 - Construction Strategies
 - Demand Management
 - Alternate Routes (or Detours)
- ▶ Prior to the start of construction, Permittee shall provide the TMP to the Director of Environmental Planning for review and approval. The TMP shall be implemented after approval by the Port. The TMP will be updated, as needed, throughout the duration of construction.

3.1.18 Tribal Cultural Resources

ENVIRONMENTAL CHECKLIST

Tribal Cultural Resources

Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
a) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or	Yes	No	No	N/A
b) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

While Tribal Cultural Resources was recently added to the Appendix G CEQA Checklist and not addressed as a separate section of the Pier B EIR, Section 3.12 addresses potential impacts to Native American resources. The certified EIR identified tribal cultural resources impacts, as follows:

- No known tribal cultural resources are located within or near the Project site, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The intersection of Anaheim Way and Farragut Street is within the Pier B Project footprint and, similar to the Pier B Project, is located in a fully developed highly industrial area and no known tribal cultural resources were identified within or near the project site. Therefore, as with the Pier B Project, construction activities would not reasonably be expected to disturb, damage, or degrade tribal cultural resources, and no mitigation is required. In addition, similar to the Pier B Project, because the potential for damaging unknown tribal cultural resources is remote, damage to or destruction of ethnographic resources considered significant to contemporary Native Americans is also not expected. As with the Pier B Project, the proposed Project would be constructed in accordance with the Pier B EIR Special Condition entitled Discovery of Archaeological Materials or Human Remains. No new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Tribal Cultural Resources were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

However, the EIR included Special Conditions for certain resource areas, including tribal cultural resources, which are identified in Section 6.3.6 (page 6-7). Although the potential for disturbing unknown prehistoric remains is remote, standard procedures would apply if unexpected discoveries occur during construction to address potential discovery of subsurface cultural materials, and include the following:

- ▶ In the unlikely event that any archaeological material is discovered during construction, Permittee shall halt all work within the vicinity of the archaeological discovery until a qualified archaeologist completes an assessment detailing the significance of the find. If the resources are found to be significant, they shall be avoided or mitigated consistent with State Office of Historic Preservation (OHP) Guidelines. Treatment plans must be developed in consultation with the county, OHP, and local Native Americans.
- ▶ If human remains are encountered during earth-moving activities, the Los Angeles County coroner shall be contacted immediately. If the remains appear to be Native American, the coroner shall contact the Native American Heritage Commission (NAHC), which will appoint the Most Likely Descendent. Additionally, if the human remains are determined to be Native American, a plan will be developed regarding the treatment of human remains and associated burial objects. This plan will be implemented under the direction of the Most Likely Descendent.
- ▶ Permittee shall immediately notify the Director of Environmental Planning of any discoveries.

3.1.19 Utilities and Service Systems

ENVIRONMENTAL CHECKLIST

UTILITIES & SERVICE SYSTEMS	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
Would the Project...				
a) Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	Yes	No	No	N/A
b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	Yes	No	No	N/A
c) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	Yes	No	No	N/A
d) Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	Yes	No	No	N/A
e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	Yes	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

Section 3.11 of the Pier B EIR analyzes potential impacts of the Pier B Project on utilities (electricity, natural gas and water) and service systems (sewer, stormwater, telephone, oil lines and solid waste). The certified EIR identified utilities and services impacts, as follows:

- ▶ Proposed Project construction activities would require the relocation and reorganization of various water, wastewater, storm drains, natural gas, electrical utility lines and infrastructure, and oil lines within the Project site. While demolition and construction of utility infrastructure would occur with the proposed Project, there would be no additional demands on the existing utilities. Demolition of existing utility infrastructure and construction of new infrastructure would be conducted in a manner designed to prevent service interruptions for adjacent tenants. Any new construction would be in conformance with current design standards such that effects on utilities and service systems would be less than significant, and mitigation is not required.
- ▶ Because it is estimated that a maximum of 10 workers per shift would be required for operation of the proposed Project, the increase in water and sewer demand would be minimal. There would be a minimal increase in electrical consumption.

- ▶ Project construction and operational activities would not exhaust or exceed existing water, wastewater, or landfill capacities; therefore, effects on utilities and service systems would be less than significant, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. Construction of the intersection realignment could require the relocation and reorganization of utilities. However, service interruptions would be prevented, and construction would not increase demands for expanded utility services. Operations would also not require any increase in utility generation demands. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Utilities and Services were identified in the Pier B Final EIR. No mitigation would be required for the Proposed Project.

3.1.20 Wildfire

ENVIRONMENTAL CHECKLIST AND DISCUSSION

Wildfire	Impact Examined in Final EIR?	Does the Project Involve New or Substantially More Severe Significant Impacts?	Do Any New Circumstances Involve New or Substantially More Severe Significant Impacts?	Do Mitigation Measures in the Final EIR Address/ Resolve Impacts, Including Impacts That Would Otherwise be New or Substantially More Severe?
If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project...				
a) Substantially impair an adopted emergency response plan or emergency evacuation plan?	Yes	No	No	N/A
b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	No	No	No	N/A
c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	No	No	No	N/A
d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	No	No	No	N/A

SUMMARY OF FINAL EIR FINDINGS

The Pier B EIR does not address wildfire as it was added to the CEQA Appendix G after the Pier B EIR was certified. However, Section 3.9, Hazards and Hazardous Materials, addresses potential impacts regarding impairment or interference with emergency response or evacuation plans. The certified EIR identified emergency response and evacuation impacts, as follows:

- ▶ Local agency requirements would be incorporated into construction planning and appropriate response procedures would be established as required by law. Contractors and the railroads would continue to comply with all emergency response and evacuation regulations. The Project would not impair or interfere with emergency response or evacuation plans. Impacts would be less than significant, and mitigation measures are not required.
- ▶ The proposed Project would be incorporated into existing emergency response plans; management of emergency response and evacuation systems would continue to be managed. Standard security measures would be implemented during Project operation and access to the Joint Command and Control Center services would not be impeded. Adequate safeguards and appropriate response procedures would be in place during Project operation, so impacts related to implementation of or interference with an adopted emergency response or evacuation plan would be less than significant, and mitigation measures are not required.

IMPACTS OF THE PROPOSED PROJECT

Reconstruction of Anaheim Way on a new alignment was identified within Phase 1 construction of the Pier B Project and would not result in new or substantially more severe impacts that have not already been addressed in the Pier B EIR. The project area is not located within or near any fire hazard zones and adequate access for emergency services would be maintained. Therefore, no new or substantially more severe impacts would occur as a result of the proposed Project.

APPLICABLE MITIGATION MEASURES AND SPECIAL CONDITIONS

No mitigation measures for Wildfire are required for the Proposed Project.

4 CONCLUSIONS

As described in Chapter 2 of this document, "Project Description," and Chapter 3, "Impact Analysis," none of the conditions described in CEQA Guidelines Section 15162 calling for preparation of a subsequent document have occurred. As documented throughout the environmental checklist and discussion, the proposed project would:

- ▶ not result in any new significant environmental effects, and
- ▶ not substantially increase the severity of previously identified significant effects.

In addition, no new information of substantial importance has arisen that shows that:

- ▶ the Project would have new significant effects,
- ▶ the Project would have substantially more severe effects,
- ▶ mitigation measures or alternatives previously found to be infeasible would in fact be feasible, or
- ▶ mitigation measures or alternatives that are considerably different from those analyzed in the EIR would substantially reduce one or more significant effects on the environment.

This Technical Memorandum confirms that the proposed intersection and roadway improvements at Anaheim Way and Farragut Street in the City of Los Angeles are within the scope of the certified Pier B Project EIR; the environmental effects of the Project were covered in the previous EIR, no new environmental effects not identified in the previous EIR will occur, no new mitigation measures are required, and all feasible mitigation measures from the previous EIR have been incorporated into the Project.

Pier B On-Dock Rail Support Facility Project

Mitigation Monitoring and Reporting Program

Prepared by



The Port of Long Beach
4801 Airport Plaza Drive
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January 2018

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Pier B On-Dock Rail Support Facility Project

Mitigation Monitoring and Reporting Program

INTRODUCTION

This Mitigation Monitoring and Reporting Program (MMRP) has been prepared for the Pier B On-Dock Rail Support Facility Project (Project) in the City of Long Beach (COLB) and City of Los Angeles (COLA). This MMRP fulfills the requirements of California Public Resources Code (PRC) Section 21081.6 and California Environmental Quality Act (CEQA) Guidelines Section 15097. As stated in PRC Section 21081.6(a)(1):

The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of approval, adopted in order to mitigate or avoid significant effects on the environment.

The primary purpose of the MMRP is to ensure that the mitigation measures identified in the Final Environmental Impact Report (EIR) for the Pier B On-Dock Rail Support Facility Project are implemented to reduce or avoid identified environmental effects and to appropriately assign the mitigation responsibilities for implementing the proposed Project. If the Project is approved, the mitigation measures listed in this MMRP will be adopted by the Port of Long Beach (POLB or Port) Board of Harbor Commissioners (Board) as a condition of Project approval. The mitigation measures would be a mandatory component of the Harbor Development Permit (HDP) for this Project.

RESPONSIBLE PARTY

The POLB is the lead agency for the Pier B On-Dock Rail Support Facility Project under CEQA; therefore, it is responsible for administering and implementing the MMRP. The Port, or its designee, will be responsible for:

- Implementing and reporting mitigation measures in this program;
- Ensuring that mitigation measures are accomplished in an environmentally responsible manner;
- Ensuring that the status of mitigation measures is reported in accordance with this program;
- Ensuring that the cost of mitigation is included in its budget;
- Ensuring that mitigation measures are properly carried out by designated and qualified personnel, which may include specialty contractors; and
- Program oversight.

Mitigation measures will be included in applicable Requests for Proposals (RFP), specifications, plans, drawings, and procedures issued for construction of the Pier B On-Dock Rail Support Facility and during operation of this facility. When Project work is undertaken by the Port's contractors, the pertinent mitigation measures will be included in the terms and conditions of the contracts. Port construction inspectors will undertake regular inspections of the job site to ensure that contractors are implementing the mitigation measures and complying with their contract. The Port's assigned Project Manager will be responsible for ensuring that mitigation measures that are the responsibility of the Port are carried out. Mitigation measures are summarized on Table 1.

Table 1. Summary of Mitigation Measures

<i>Air Quality and Health Risk</i>	
1	Mitigation Measure AQ-1: On-Road Construction Trucks. All on-road heavy-duty trucks with a fifth-wheel tractor/trailer and a gross vehicle weight rating (GVWR) of 19,500 pounds or more transporting materials to and from the construction site shall meet United States Environmental Protection Agency (EPA) 2010 on-road heavy-duty diesel engine emission standards.
2	Mitigation Measure AQ-2: Tier 4 Construction Equipment. All self-propelled, diesel-fueled off-road construction equipment 25 horsepower (hp) or greater shall meet EPA/California Air Resources Board (CARB) Tier 4 off-road engine emission standards.
3	<p>Mitigation Measure AQ-3: Off-Road Construction Equipment. Off-road diesel-powered construction equipment shall comply with the following:</p> <ul style="list-style-type: none"> • Maintain all construction equipment according to manufacturer's specifications. • Construction equipment shall not idle for more than 5 minutes when not in use. • High-pressure fuel injectors shall be installed on construction equipment vehicles. <p>The benefits to be achieved by the above-listed components of this measure were not quantified in the analysis due to the wide range of variables involved. This measure is applied, however, to further reduce combustion emissions.</p>
4	Mitigation Measure AQ-4: Increased Watering Frequency for Fugitive Dust Control. Construction site watering, required by SCAQMD Rule 403, shall be increased such that the watering interval is no greater than 2.1 hours. This measure would increase the fugitive dust emissions control from 61 to 74 percent.
5	<p>Mitigation Measure AQ-5: Additional Fugitive Dust Control. Contractors shall:</p> <ul style="list-style-type: none"> • Apply approved nontoxic chemical soil stabilizers according to manufacturers' specifications to all inactive construction areas or replace groundcover in disturbed areas. • Provide temporary wind fencing around sites being graded or cleared. • Cover truck loads that haul dirt, sand, or gravel or maintain at least 2 feet of freeboard in accordance with Section 23114 of the California Vehicle Code. • Install wheel washers where vehicles enter and exit unpaved roads onto paved roads, or wash off tires of vehicles and any equipment leaving the construction site. • Suspend all soil disturbance activities when winds exceed 25 miles per hour (mph) or when visible dust plumes emanate from the site and stabilize all disturbed areas. <p>The benefits to be achieved by the above-listed components of this measure were not quantified in the analysis due to the wide range of variables involved. This measure is applied, however, to further reduce fugitive dust emissions.</p>
6	Mitigation Measure AQ-6: Cumulative Air Quality Impact Reduction Program. To reduce air quality impacts associated with operation, the Port will contribute to the Community Grants Program (CGP). For the proposed Project, the contribution to the CGP would be \$149,757 total.
<i>Biota and Habitats</i>	
7	Mitigation Measure BIO-1: Protection of Bats. A qualified bat specialist shall conduct a preconstruction survey. If bats are found or determined to be potentially present, construction activity will be stopped if determined to be disruptive to breeding or roosting, and appropriate subsequent actions will be identified and implemented.
8	Mitigation Measure BIO-2: Protection of Migratory Birds. Construction activities that could remove trees or structures that may support the nests of protected birds will follow the requirements of the Migratory Bird Treaty Act (MBTA). Specific procedures will be identified by a qualified ornithologist and implemented.
<i>Cultural Resources</i>	
9	Mitigation Measure CR-1: Paleontological Monitoring. A paleontological monitoring program shall be implemented during earthmoving that requires excavation at or below 5 feet of depth, or where fossiliferous or older alluvium material is encountered.

Table 1. Summary of Mitigation Measures (Cont'd)

<i>Cultural Resources (Cont'd)</i>	
10	Mitigation Measure CR-2: Inadvertent Discovery of Paleontological Resources. In the event that construction activities encounter potentially fossiliferous materials, work in the immediate vicinity will be temporarily halted until a qualified vertebrate paleontologist can evaluate the discovery and implement appropriate treatment measures.
<i>Global Climate Change</i>	
11	Mitigation Measure GCC-1: LEED. If new buildings constructed as part of the proposed Project meet COLB Green Building Policy criteria, Leadership in Energy and Environmental Design (LEED) certification shall be sought. COLB exempts buildings of less than 7,500 square feet of occupied space from its Green Building Policy.
12	Mitigation Measure GCC-2: Recycling of Construction Materials. Pursuant to the POLB Administrative Directive (Sustainable Business Practices), construction debris must be recycled, reused or otherwise diverted from landfills to the maximum extent possible. Recyclable construction waste generated by the Project shall be taken to an accredited recycling center.
13	Mitigation Measure GCC-3: Recycling and Sustainable Business Practices. During operation, the Port shall follow recycling objectives and measures established by the Port's Administrative Directive (Sustainable Business Practices) (POLB, 2006). In general, products made with recycled materials require less energy and raw materials to produce than products made with unrecycled or raw materials. This mitigation measure also includes energy conservation practices, purchasing of "Green" products, energy-efficient lighting, low-volatile organic compound (VOC) paint and finishes, and use of recycled or remanufactured carpeting and office furnishings. This directive also includes minimizing the use of paper and plastic, reusing materials and equipment, and proper disposal of alkaline batteries. The effectiveness of this mitigation measure was not quantified due to the lack of a standard emission estimation approach.
14	Mitigation Measure GCC-4: Xeriscaping. Water conservation features, including drought-tolerant plant materials, are required for all projects undertaken in the Port. Xeriscape landscaping shall incorporate the use of water conservation features including, but not limited to, drought-tolerant plants; hardscape; permeable material such as concrete, asphalt, and pavers; recycled material such as concrete, gravel, granite, and shredded redwood; and drip irrigation systems and timers.
15	Mitigation Measure GCC-5: Tree Planting. The Port shall plant shade trees around the main office and maintenance buildings in accordance with species identified in the Green Port of Long Beach Sustainable Landscape Palette and POLB Sustainable Development Guidelines. Although not quantified, implementation of this measure is expected to reduce the Project's GHG emissions by less than 0.1 percent.
16	Mitigation Measure GCC-6: Tree Planting – Transportation Corridors. The Port shall plant new shade trees on Port-controlled lands adjacent to the roads that lead into the facility, to the extent practicable, consistent with safety and other land use considerations. The effectiveness of this mitigation measure was not quantified due to the lack of a standard emission estimation approach.
17	Mitigation Measure GCC-7: Employee Carpooling. The construction contractor and the Port shall encourage construction and facility employees to carpool or to use public transportation. These employers shall provide incentives to promote the measure, such as preferential parking for carpoolers or vanpool subsidies, and they shall provide information to employees regarding the benefits of alternative transportation methods. The effectiveness of this mitigation measure was not quantified due to the lack of a standard emission estimation approach.
18	Mitigation Measure GCC-8: Community Grants Program (CGP). The Port will implement and fund the CGP to partially address the cumulative GHG impacts of the proposed Project. The Port shall provide \$1.4 million, as determined by the POLB CGP funding level methodology.

Table 1. Summary of Mitigation Measures (Cont'd)

<i>Global Climate Change (Cont'd)</i>	
19	<p>Mitigation Measure GCC-9: Indirect GHG Emission Avoidance and Mitigation. The Port shall minimize indirect GHG emissions through measures that reduce or avoid electricity consumption at the facility. Such measures may include, but are not limited to, the use of low-energy demand lighting (e.g., fluorescent or light-emitting diode [LED]), and use of energy-efficient floodlights.</p> <p>To identify future opportunities to reduce indirect GHG emissions, the Port shall conduct a third-party energy audit every 5 years and install innovative power-saving technologies where feasible, such as power factor correction systems and lighting power regulators. Such systems help to maximize usable electric current and eliminate wasted electricity, thereby lowering overall electricity use.</p>

2 **APPLICABILITY OF MITIGATION MEASURES TO PROJECT ALTERNATIVES**

3 While it is not known at this time which of the Project alternatives, if any, would be approved
 4 by Board of Harbor Commissioners, approval of the Project will be contingent upon a
 5 commitment to accomplishing the mitigation measures identified in the Final EIR. While the
 6 severity of environmental impacts may vary depending on the alternative to be implemented,
 7 all mitigation measures applicable to the proposed Project (12th Street Alternative) are also
 8 applicable to the 10th Street Alternative and 9th Street Alternative as well as design variations
 9 of the 12th Street and 10th Street Alternatives.

10 **MITIGATION MONITORING AND REPORTING PROGRAM PROCEDURES**

11 The designated POLB Environmental Monitor assigned to the Pier B On-Dock Rail Support
 12 Facility Project, or Designee, will track and document compliance with mitigation measures,
 13 note any problems that may result, and take appropriate action to remedy problems. Specific
 14 responsibilities of the POLB Environmental Monitor or Designee are:

- 15 • Coordination of all mitigation monitoring activities;
- 16 • Management of the preparation, approval, and filing of monitoring or permit compliance
17 reports;
- 18 • Maintenance of records concerning the status of all mitigation measures;
- 19 • Retaining a file containing documentation of the completion of all mitigation measures;
- 20 • Quality control assurance of field monitoring personnel;
- 21 • Coordination with regulatory agencies for compliance with mitigation and permit
22 requirements;
- 23 • Reviewing and recommending acceptance and certification of implementation
24 documentation;
- 25 • Serving as the point of contact for interested parties or surrounding property owners who
26 wish to register complaints; and
- 27 • Documenting observations of unsafe conditions or environmental violations, and
28 identifying any necessary corrective actions.

MITIGATION AND MONITORING REPORTING PLAN COMPLETION FORMS

The MMRP includes a Completion Form for each mitigation measure shown on a separate page. For each mitigation measure, the MMRP Completion Form identifies the following:

- Required action;
- When the action is required to be taken;
- Agency responsible for action;
- Agency responsible for tracking the action;
- Specific action(s) to ensure implementation of the mitigation measure;
- Submittal date;
- Person verifying implementation (name and title);
- Attachments required to verify implementation; and
- Comments made by verifying personnel.

The agency responsible for taking the action (i.e., POLB Engineering Services) will submit the appropriate completion form with attachments to the agency responsible for tracking the action (POLB Planning Division). By his or her signature, the POLB Planning Division representative verifies that each mitigation measure has been implemented.

MITIGATION AND MONITORING ANNUAL REPORTING

This MMRP will require an annual report within the first year of Project approval (including during design activities) and then annually thereafter. The MMRP will document compliance with implementing the mitigation measures included in the Final EIR, Project HDP and construction contracts.

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1 **Pier B On-Dock Rail Support Facility Project**

2 Mitigation Monitoring and Reporting Program Completion Forms

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Mitigation Measure AQ-1: On-Road Construction Trucks

Required Action: All on-road heavy-duty trucks with a fifth-wheel tractor/trailer and a gross vehicle weight rating (GVWR) of 19,500 pounds or more transporting materials to and from the construction site shall meet United States Environmental Protection Agency (EPA) 2010 on-road heavy-duty diesel engine emission standards.

When Required: Daily during all construction activities.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management and Environmental Planning Division.

Action (i): POLB Engineering Services to include this requirement in Project construction specifications and bid process.

Action (ii): POLB Construction Management Division to verify that on-road heavy-duty trucks with a fifth-wheel tractor/trailer and a GVWR of 19,500 pounds or more have current vehicle registration and meet United States Environmental Protection Agency (EPA) 2010 on-road heavy-duty diesel engine emission standards.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure AQ-2: Tier 4 Construction Equipment

Required Action: All self-propelled, diesel-fueled off-road construction equipment 25 horsepower (hp) or greater shall meet United States Environmental Protection Agency (EPA)/California Air Resources Board (CARB) Tier 4 off-road engine emission standards.

When Required: During all construction activities.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include this requirement in Project construction specifications and bid process.

Action (ii): POLB Construction Management Division to verify that self-propelled, diesel-fueled off-road construction equipment 25 hp or greater meet United States EPA/CARB Tier 4 engine emission standards. A copy of each unit's certified tiered specification and any required CARB or South Coast Air Quality Management District (SCAQMD) operating permit will be made available at the time each piece of equipment is mobilized.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure AQ-3: Off-Road Construction Equipment

Required Action: Off-road diesel-powered construction equipment shall comply with the following:

- Maintain all construction equipment according to manufacturer's specifications.
- Construction equipment shall not idle for more than 5 minutes when not in use.
- High-pressure fuel injectors shall be installed on construction equipment vehicles.

When Required: Daily during all construction activities.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include requirements in Project construction specifications and bid process.

Action (ii): Construction Management Division to verify that off-road diesel-powered construction equipment are in good maintenance condition, do not idle more than 5 minutes when in use, and that high-pressure fuel injectors are installed.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure AQ-4: Increased Watering Frequency for Fugitive Dust Control

Required Action: Construction site watering, required by SCAQMD Rule 403, shall be increased such that the watering interval is no greater than 2.1 hours. This measure would increase the fugitive dust emissions control from 61 to 74 percent.

When Required: During all construction activities involving groundwork (i.e., moving dirt).

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include these requirements in Project construction specifications and bid process.

Action (ii): POLB Construction Management Division to verify that contractor is implementing emission reduction measures including construction site watering at the above specified intervals.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure AQ-5: Additional Fugitive Dust Control

Required Action: Contractors shall:

- Apply approved nontoxic chemical soil stabilizers according to manufacturers' specifications to all inactive construction areas or replace groundcover in disturbed areas.
- Provide temporary wind fencing around sites being graded or cleared.
- Cover truck loads that haul dirt, sand, or gravel or maintain at least 2 feet of freeboard in accordance with Section 23114 of the California Vehicle Code.
- Install wheel washers where vehicles enter and exit unpaved roads onto paved roads, or wash off tires of vehicles and any equipment leaving the construction site.
- Suspend all soil disturbance activities when winds exceed 25 miles per hour (mph) or when visible dust plumes emanate from the site and stabilize all disturbed areas.

When Required: During all construction activities.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include each of the above requirements in Project construction specifications and bid process.

Action (ii): POLB Construction Management Division to verify that each of the above requirements are carried out during each construction phase.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure AQ-6: Cumulative Air Quality Impact Reduction Program

Required Action: To reduce cumulative air quality impacts associated with operation of the proposed Project, the Port shall require the Project to contribute \$149,757 to the Community Grants Program.

When Required: Within 30 days after Project Opening.

Agency Responsible for Action: POLB Environmental Planning Division.

Agency Responsible for Tracking: POLB Environmental Planning Division.

Action: POLB Environmental Planning Division to ensure the timing of the payments determined by the methodology described in the EIR be made by the later of the following two dates: (a) the date that the Port issues a Notice to Proceed (NTP) or otherwise authorizes commencement of construction on the Pier B On-Dock Rail Support Facility Project construction contract, or (b) the date that the Pier B On-Dock Rail Support Facility Final EIR is conclusively determined to be valid, either by operation of California PRC Section 21167.2 or by final judgement or final adjudication.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure BIO-1: Protection of Bats

Required Action: To avoid harm to bats from modifications to bridges that may provide roosting or breeding habitat, the following procedure will be followed:

- Prior to the start of construction on the Dominguez Channel rail bridge, a qualified bat specialist shall conduct a pre-construction bat survey of the construction work zone.
- If bats, or evidence of bats, are found or if bats are determined to be potentially present, the bridge will be inspected no more than 7 days before any disturbance to confirm the presence of roosting bats.
- The bat specialist will have authority to stop construction activity likely to be disruptive of breeding or roosting. The bat specialist would identify an appropriate course of action for the POLB to follow. Example actions are: (a) precluding bat access from the existing bridge before work proceeds; (b) establishing an appropriate buffer area; and (c) monitoring work to ensure that bats are not killed or substantially disturbed.
- Weekly reports to the POLB Environmental Planning Division and California Department of Fish and Wildlife (CDFW) shall be provided, describing monitoring actions, relevant observations, and any protective actions taken.

When Required: Prior to, and during (if warranted), construction work on or beneath the Dominguez Channel rail bridge.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include in Project construction specifications and bid process a requirement for a qualified bat specialist (biologist) to conduct a pre-construction bat survey at the Dominguez Channel rail bridge construction zone.

Action (ii): POLB Construction Management Division to verify that a pre-construction bat survey has been carried prior to construction on or beneath the Dominguez Channel rail bridge; and that bat protection measures, if warranted, are carried out during construction at this location.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure BIO-2: Protection of Migratory Birds

Required Action: To minimize effects on nesting migratory birds, construction activities that include the removal of trees, shrubs, or structures that may support the nests of protected birds will follow the requirements of the Migratory Bird Treaty Act (MBTA). If construction activities occur during the bird breeding season (February 15 through August 31), a qualified ornithologist shall survey trees, shrubs, and structures to be removed, not more than 3 days prior to removal. If the ornithologist detects any occupied nests or nesting behavior, the POLB shall conspicuously flag off the area(s) and provide a minimum buffer of 100 feet (300 feet for raptors) between the nest and limits of construction. Construction crews will be instructed to avoid any activities in this zone. Construction activities could resume within the buffer at the direction of the ornithologist when fledglings have left the nest or if the nest is abandoned.

When Required: For construction activities scheduled to occur between February 15 and August 31 of any year in areas with vegetation that may support nesting of protected birds.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include requirements for a qualified ornithologist to conduct a pre-construction bird survey in construction areas that contain trees, shrubs, and other structures that support nesting birds that would be removed.

Action (ii): In the event occupied nests are identified, or nesting behavior detected, in the construction area, POLB Engineering Services to retain a qualified ornithologist to:

- Establish a buffer zone between the nest(s) and limits of construction;
- Instruct construction crews to avoid any activities in this zone;
- Periodically monitor progress of nesting activities;
- Notify POLB Construction Management Division and the POLB Environmental Planning Division when fledglings have left the nest or if the nest is abandoned so that construction activities may resume in the affected area; and
- Prepare a written report to document monitoring activities.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure CR-1: Paleontological Monitoring

Required Action: A paleontological monitoring program shall be implemented during earthmoving that requires excavation at or below 5 feet of depth, or where fossiliferous or older alluvium material is encountered.

When Required: During any excavation at or below 5 feet of depth or where fossiliferous or older alluvium material is encountered.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to determine if any excavation at or below 5 feet of depth is required. POLB Engineering Services to also determine, based on site-specific geotechnical investigation (to be prepared), if any fossiliferous or older alluvium material will be encountered during construction.

Action (ii): For these work zones, POLB Engineering Services will include a requirement for contractor to provide a qualified vertebrate paleontologist contractor to provide paleontological monitoring services. These requirements shall be included in Project construction specifications and bid process.

Action (iii): POLB Construction Management Division to verify that selected contractor has included services of a qualified paleontologist in its contract.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure CR-2: Inadvertent Discovery of Paleontological Resources

Required Action: In the event that construction activities encounter potentially fossiliferous materials, work in the immediate vicinity will be temporarily halted until a qualified vertebrate paleontologist can evaluate the discovery and implement appropriate treatment measures.

The paleontologist would determine if the paleontological material should be salvaged, identified, and permanently preserved. Any fossils recovered will be cleaned and prepared to the point of identification, sorted, and catalogued. Prepared fossils, along with copies of all pertinent field notes, photos, and maps, will be deposited into an accredited museum repository by a qualified paleontologist, who will also prepare a report of findings for the POLB. If it can be demonstrated that the project will cause damage to these resources, reasonable efforts shall be made to permit any or all of the resource to be scientifically removed, or it shall be preserved in situ (left in an undisturbed state). In situ preservation may include the following options (or equivalent measures): amending construction plans to avoid the resources; setting aside sites containing these resources by deeding them into permanent conservation easements; capping or covering these resources with a protective layer of soil before building on the sites; incorporating green space or other open space into the project to leave these resources undisturbed and to provide a protective cover over them; and avoiding public disclosure of the location of these resources until or unless the site is adequately protected from vandalism or theft.

All fossils shall be documented in a detailed Paleontological Mitigation Report. Fossils recovered from the field or by processing shall be prepared; identified; and, along with accompanying field notes, maps, and photographs, accessioned into the collections of a designated accredited museum such as the Natural History Museum of Los Angeles County or the San Diego Natural History Museum.

When Required: During all earthwork activities and when potentially fossiliferous material is unearthed.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include a requirement for its construction contractor to provide a qualified paleontologist (on-call) in its Project construction specifications.

Action (ii): POLB Construction Management Division to ensure that selected contractor has a qualified paleontologist available as needed.

Action (iii): POLB Engineering Services to ensure that adequate funding is available for curation of fossils recovered from the construction site and preparation of a Paleontological Mitigation Report.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-1: Leadership in Energy and Environmental Design

Required Action: If new buildings constructed as part of the proposed Project meet COLB Green Building Policy criteria, Leadership in Energy and Environmental Design (LEED) certification shall be sought. COLB exempts buildings of less than 7,500 square feet of occupied space from its Green Building Policy.

When Required: During Final Design of New Buildings 7,500 square feet or more in size.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Engineering Services and Environmental Planning Division.

Action (i): POLB Engineering Services shall include a LEED certification requirement for new buildings 7,500 square feet or more in size in its Project construction specifications and bid processes.

Action (ii): POLB Engineering Services shall participate in efforts to obtain LEED certification for new buildings 7,500 square feet or more in size.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-2: Recycling of Construction Materials

Required Action: Pursuant to the POLB Administrative Directive (Sustainable Business Practices), construction debris must be recycled, reused, or otherwise diverted from landfills to the maximum extent possible. Recyclable construction waste generated by the Project shall be taken to an accredited recycling center.

When Required: During demolition and construction activities.

Agency Responsible for Action: POLB Engineering Services and Construction Management Divisions.

Agency Responsible for Tracking: POLB Construction Management Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include requirements for recycling of construction materials in its Project construction specifications and bid processes.

Action (ii): POLB Construction Management Division to ensure that construction materials are being recycled during demolition and other construction activities.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-3: Recycling and Sustainable Business Practices

Required Action: During operation, the Port shall follow recycling objectives and measures established by the Port's Administrative Directive (Sustainable Business Practices). In general, products made with recycled materials require less energy and raw materials to produce than products made with unrecycled or raw materials. This mitigation measure also includes energy conservation practices, purchasing of "Green" products, energy-efficient lighting, low-volatile organic compound (VOC) paint and finishes, and use of recycled or remanufactured carpeting and office furnishings. This directive also includes minimizing the use of paper and plastic, reusing materials and equipment, and proper disposal of alkaline batteries.

When Required: During Operation of the Pier B Rail Yard.

Agency Responsible for Action: POLB and Pacific Harbor Line (PHL).

Agency Responsible for Tracking: POLB Environmental Planning Division.

Action (i): POLB Engineering Services to include requirements for recycling objectives and measures in its Project construction specifications and bid processes.

Action (ii): POLB Environmental Planning Division shall ensure that PHL is practicing recycling objectives and measures, to the extent feasible and practical, in routine operation of the rail yard.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-4: Xeriscaping

Required Action: Water conservation features, including drought-tolerant plant materials, are required for all projects undertaken in the Port. Xeriscape landscaping shall incorporate the use of water conservation features including, but not limited to, drought-tolerant plants; hardscape; permeable material such as concrete, asphalt, and pavers; recycled material such as concrete, gravel, granite, and shredded redwood; and drip irrigation systems and timers.

When Required: During Project Design (prior to acceptance of Final Design).

Agency Responsible for Action: POLB Engineering Services and Construction Management Divisions.

Agency Responsible for Tracking: POLB Engineering Services and Environmental Planning Division.

Action (i): POLB Engineering Services to include xeriscape landscaping in Project construction specifications and bid process.

Action (ii): POLB Construction Management Division to verify that xeriscape landscaping is installed in accordance with construction specifications.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-5: Tree Planting

Required Action: The Port shall plant shade trees around the main office and maintenance buildings in accordance with species identified in the Green Port Long Beach Sustainable Landscape Palette and POLB Sustainable Development Guidelines.

When Required: During Project Design (prior to acceptance of Final Design) and During Construction.

Agency Responsible for Action: POLB Engineering Services and Construction Management Divisions.

Agency Responsible for Tracking: POLB Engineering Services, Maintenance Division and Environmental Planning Division.

Action (i): POLB Engineering Services to include planting of shade trees in Project construction specifications and bid process for main office and maintenance buildings.
Action (ii): POLB Construction Management Division to verify that planting of shade trees is accomplished in accordance with construction specifications.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-6: Tree Planting – Transportation Corridors

Required Action: The Port shall plant new shade trees on Port-controlled lands adjacent to the roads that lead into the facility, to the extent practicable, consistent with safety and other land use considerations.

When Required: During Project Design (prior to acceptance of Final Design) and During Construction.

Agency Responsible for Action: POLB Engineering Services and Construction Management Divisions.

Agency Responsible for Tracking: POLB Engineering Services and POLB Environmental Planning Division.

Action (i): POLB Engineering Services to include planting of shade trees (along roadways) in Project construction specifications and bid process for main office and maintenance buildings.

Action (ii): POLB Construction Management Division to verify that planting of shade trees (along roadways) is accomplished in accordance with construction specifications.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-7: Employee Carpooling

Required Action: The Port and construction contractors shall encourage construction and facility employees to carpool or to use public transportation. These employers shall provide incentives to promote the measure, such as preferential parking for carpoolers or vanpool subsidies, and they shall provide information to employees regarding the benefits of alternative transportation methods.

When Required: During Project construction and operations.

Agency Responsible for Action: POLB Engineering Services and Construction Management Divisions.

Agency Responsible for Tracking: POLB Environmental Planning Division.

Action (i): POLB Engineering Services to include requirements for employee carpooling and use of public transportation in its Project construction specifications and bid processes.

Action (ii): POLB Construction Management Division to ensure that employee carpooling and use of public transportation is encouraged during demolition and construction activities.

Action (iii): POLB Environmental Planning Division shall ensure that PHL is encouraging employee carpooling and use of public transportation, to the extent feasible and practical, in routine operation of the rail yard.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-8: Community Grants Program

Required Action: The Port will implement and fund the Community Grants Program (CGP) to partially address the cumulative greenhouse gas (GHG) impacts of the proposed Project. The Port shall provide \$1.4 million, as determined by the POLB CGP funding-level methodology.

When Required: Within 30 days after Project Opening.

Agency Responsible for Action: POLB Environmental Planning Division.

Agency Responsible for Tracking: POLB Environmental Planning Division.

Action: POLB Environmental Planning Division to ensure the timing of the payments determined by the methodology described in the EIR be made by the later of the following two dates: (a) the date that the Port issues a Notice to Proceed (NTP) or otherwise authorizes commencement of construction on the Pier B On-Dock Rail Support Facility Project construction contract, or (b) the date that the Pier B On-Dock Rail Support Facility Final EIR is conclusively determined to be valid, either by operation of California PRC Section 21167.2 or by final judgement or final adjudication.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

Mitigation Measure GCC-9: Indirect GHG Emission Avoidance

Required Action: The Port shall minimize indirect greenhouse gas (GHG) emissions through measures that reduce or avoid electricity consumption at the facility. Such measures may include, but are not limited to, the use of low-energy demand lightings (e.g., fluorescent or light-emitting diode [LED]), and use of energy-efficient floodlights.

To identify future opportunities to reduce indirect GHG emissions, the Port shall conduct a third-party energy audit every 5 years and install innovative power-saving technologies where feasible, such as power factor correction systems and lighting power regulators.

When Required: During facility engineering and design and prior to acceptance of final design drawings. In addition, an energy audit would be conducted 5 years after operation initiates at new facilities.

Agency Responsible for Action: POLB Engineering Services.

Agency Responsible for Tracking: POLB Engineering Services, Construction Management Division, and Environmental Planning Division.

Action (i): POLB Engineering Services to include requirements for measures that reduce or avoid electricity consumption in Project construction specifications and bid process.

Action (ii): POLB Construction Management Division to verify that energy conservation measures have been installed in accordance with construction specifications.

Action (iii): POLB Engineering Services and Environmental Division to ensure that a third-party energy audit is conducted every 5 years after the start of facility operations, and that innovative power-saving technologies are implanted and installed where feasible.

Submittal Date:

Verified By:

Title:

Attachments:

Comments:

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The following attachments are available on the Port of Long Beach website at:

www.polb.com/ceqa

Attachment 4	Final EIR – Pier B On-Dock Rail Support Facility Project
Attachment 5	Draft EIR – Pier B On-Dock Rail Support Facility Project
Attachment 6	Draft EIR – Pier B On-Dock Rail Support Facility Project Appendices

EXHIBIT E

PUBLIC CORRESPONDENCE



Wilmington Neighborhood Council

544 N. Avalon Blvd., Suite 103, Wilmington CA 9074

(310) 522-2013 WilmingtonNC@empowerLA.org
Wilmingtonneighborhoodcouncil.com

*Gina Martinez, Chair
Steve Contreras, Vice-Chair
Christina Dake, Secretary
Samantha Martinez, Treasurer
Santiago Sedillo, Parliamentarian*

May 25, 2021

Mayor of Long Beach, Robert Garcia
Mayor of Los Angeles, Eric Garcetti
Port of Long Beach, Harbor Commissioners
Port of Long Beach, Environmental Planning Department
Los Angeles City, Planning Department
Los Angeles City Council
Congresswoman, Nanette Barragan
AQMD

Subject: CF 19-0739, Coastal Permit Case# DIR-2020-7285-CDP

Oppose the Long Beach Port, Pier B On-Dock Rail Support Facility Project

Dear Honorable Leaders,

The Wilmington Neighborhood Council Governing Board held a public Brown Act meeting to discuss the Port of Long Beach, Pier B On-Dock Rail Support Facility Project. After a presentation was given by the Port of Long Beach, the WNC Planning & Land Use committee reviewed the documentation from the Los Angeles City Planning Department. Upon further review we recommend the following:

We oppose this current and ongoing Long Beach Pier B On-Dock Rail Support Facility Project that will have severe impacts on our stakeholders and take land away from our community. Although presentations were given to the Neighborhood Councils in San Pedro with requests for letters of support for this project, this rail project does not go through the community of San Pedro at all. The project only goes through the community of Wilmington which is completely separate from San Pedro. The location of this expansion flows into Wilmington from the Long Beach borders. San Pedro will not hear or feel the impacts of this project. They are approximately 6.8 miles away on the other side of the bay.

Here is the summary of our cost benefit analysis:

Costs to the Wilmington Community:

The Final EIR concluded that the project would pose "significant and unavoidable air quality and health risks and greenhouse gas emission would remain higher than the SCAQMD threshold". (File #:HD-18-034, Version: 1, page 5 of 6)

The 24-hour sound emissions and ground vibrations from this project may “average out” to be within the Federal standards, but this does not reflect the reality of the negative impacts of sleep deprivation on a community that is already living in one of the most challenging environments in our city.

There will be increased fire and explosion risks due to the refueling of locomotives.
Wilmington will be losing more land that could be used for local businesses and jobs.

Benefits to the Wilmington Community:

The project will deposit \$1.45 million over seven years to the special fund for mitigation. This is only 0.16% of the project’s \$900M budget and can only be accessed through a competitive grant process. In comparison, other commercial development projects in the City of LA must contribute 1% toward public art.

The general “more jobs” benefit was mentioned during the presentation. However, based on experience most of those working in port related jobs choose to live and shop outside of Wilmington.

Conclusion

The costs of this project outweigh the benefits for our community of Wilmington. The idea that a port expansion project of this magnitude would not proactively identify and mitigate its negative impacts on our community is appalling.

Recommendations

Port expansion projects should include a community impact and mitigation study conducted by an independent third-party expert. The Ports have not demonstrated the ability to fully understand and mitigate the negative impacts to our Wilmington community. A third-party expert is necessary to identify what mitigation is necessary and to propose the best use of funds to offset the negative impacts of the port expansion. Determining the solutions for mitigation should not be put on the community and the funding should not be doled out through competitive grants. The ports should proactively think like a community member that raises their families here and needs to bear the endless noise, traffic, pollution, blight, and then crime and drug use that festers out of these conditions.

Here are some examples of the types of mitigation measures that a third-party expert may determine to be appropriate:

- Double pane windows
- HVAC systems with high quality filtration systems
- Renewable energy systems to power the HVAC systems
- Code enforcement for port related traffic and storage
- Creating more buffer zones between residential and all port related industrial activities
- Recurring periodic cleaning of homes and vehicles of port related industrial dust

Industrial commerce, in the name of economic development for the city, State and Nation, directly leads to hazards in our community such as, crumbling roads and streets, port truck traffic, noise, air, trains, water and land pollution. This community is a coastal community with contaminated ocean waters, no safe beach access or community coastal access. No views due to the increased Port growth, unsightly

container storage yard, and port cranes. Our underprivileged community is in great need of basic resources. We are now blighted by the Port's impacts.

These issues intersect with our daily lives and causes unfair burdens. It all makes for a more dangerous and unsafe community. The health and well-being of those who live here are in great danger of contracting asthma, bronchitis, even cancer. Residents who live in Wilmington have cancer among other health issues due to the environmental hazardous directly related to port businesses. Wilmington is one of the Nation's most polluted communities and only one of three communities in Los Angeles who fall under the Clean up Green up ordinance. All due to the port and related businesses.

The goal to eliminate the number of trucks on the road and increase containers to be loaded onto trains can never be totally achieved. (30%) of cargo is loaded onto trains. The cargo that is loaded onto these rail cars is cargo going to the center of our Nation. It will always be the smaller percentage of cargo received from vessels docked in our ports.

"Local" cargo discharged from port vessels are trucked (70% of cargo) and it is the largest percentage in volume that can never be eliminated. This trucked cargo goes to local cities and they will always need supplies which are labeled "Local Loads" going to nearby cities and even neighboring states such as Arizona, Nevada and Utah. This trucked cargo will still go through our communities, and as the Port grows, so does the trucked cargo.

Community impacts

- The Environmental Impact Report states that **pollution levels will increase.**
- The Environmental Impact Report states that the **impacts are great and unmeasurable,**
- The Environmental Impact Report states that the **impacts are significant and unavoidable.**
- The city of Los Angeles directly mitigates impacts from LAX with local stakeholders. This is a transportation mitigation and should be handled in a similar way. Direct.
- The Ports of LA/ LB and the cities of Los Angeles & Long Beach continue to reap the monetary benefits without proper community mitigation
- Wilmington sits on the third largest oil field in the Nation, subject to methane gas and oil wells.
- The project report states that 30 locomotives will be fueled in this area which poses a safety hazard. With several large refineries, the impacts are deadly and **unwelcomed** by stakeholders who travel this area.
- Our low-income community of color is overburdened.

The Port of Long Beach has been enjoying the benefits of the Port expansion with record breaking numbers each year. The May 2021 report states that the Port of Long Beach had the strongest April in history with a 43 percent increase and for the 10th consecutive month the Port has broken monthly cargo movement records. With these figures, the Port's economy is booming.

Our Nation's reliance on maritime transportation and international trade remains unchanged as there is the essential need for cargo to move through our ports. The maritime industry has kept our supply chain functioning and our economy strong but it is time to finally take responsibility, time to address the EIR effects imposed on our community. It is time to address the health risk associated with living next to a Port industrial complex. Further it is time to improve addressing all these issues.

Action

Due to the harmful impacts the project has on our community:

- 1) We urge LA City Council to **deny the Coastal Development Permit**
- 2) We ask LA City Council to **assert jurisdiction** over this matter and address these serious concerns
- 3) Please ask the port to hire a third-party expert who can identify what mitigation is necessary and to propose the best use of funds to offset the negative impacts of the port expansion directly. Determining the solutions for mitigation should **not** be put on the community of Wilmington.
- 4) We ask for a moratorium to be placed for the next 10-20 years to collect data on the extent of the impacts this project is having on our community.

As the duly elected body, by way of the city charter, the Wilmington Neighborhood Council is grateful for the opportunity to advocate. We are recognized as volunteer elected officials as we serve both the City of Los Angeles and our community through the Neighborhood Council System.

Respectfully Submitted,

A handwritten signature in black ink that reads "Gina Martinez". The script is cursive and fluid.

Gina Martinez, Chair of the Wilmington NC
On Behalf of the Wilmington Neighborhood Council



Connie Chauv <connie.chauv@lacity.org>

Wilmington Neighborhood Council Board action on Port of Long Beach Pier B rail expansion

Valerie Contreras <valcwnc@gmail.com>

Wed, Mar 1, 2023 at 4:29 PM

To: Alison.spindler-Ruz@longbeach.gov, Jackie Garcia <Jackie.Garcia@lacity.org>, "tim.mcosker@lacity.org" <tim.mcosker@lacity.org>

Bcc: connie.chauv@lacity.org

Greetings!

On behalf of the Wilmington Neighborhood Council, we are sending the attached letter and hearing information to: The Mayor of Long Beach, Rex Richardson, Mayor of Los Angeles, Karen Bass, Port of Long Beach, Harbor Commissioners, Port of Long Beach, Environmental Planning Department, Los Angeles City Planning Department, Congresswoman, Nanette Barragan, Councilmember Tim McOsker CD15.

Please be advised that this is an ongoing project that our council has been working on since 2017 and many elected officials are new and can be briefed on this important project that will negatively impact our stakeholders and commuters who travel through our community. We do not have the infrastructure for the growing Ports and the impact they have on Wilmington. Rails from both the Los Angeles terminals and the Long Beach terminals will travel via this rail expansion. Our Board and Planning & Land Use committee worked extensively on this project over several years before giving this input.

We ask you to support our efforts to oppose this project for the sake of our community. Who can we call on? if not you for equality, for our disadvantaged community for inclusion, economic and environmental justice.

Thank you for the opportunity to address you on behalf of our community.

--

Best Regards,

*Valerie Contreras, President of the WNC
Wilmington Neighborhood, a 98% Latin Community of Color*

2 attachments



05-25-21-Letter on the Port of Long Beach Pier B On-Dock project CF 19-0739 (2).pdf
204K



CPC-2020-7285_HearingNotice (1).pdf
613K



Connie Chauv <connie.chauv@lacity.org>

(no subject)

Brian Mello <MelloB@agc-ca.org>

Fri, Apr 14, 2023 at 2:58 PM

To: "connie.chauv@lacity.org" <connie.chauv@lacity.org>

Cc: "matthew.lyman@polb.com" <matthew.lyman@polb.com>, Suzanne Scheideker Cook
<strategicventuresbuild@gmail.com>, "Lambrigger, Darrin" <darrin.lambrigger@polb.com>

Good afternoon,

Please see attached a letter of support on behalf of AGC of California. Please let me know if there are any questions.

Thanks,
Brian

Brian Mello
Associate Vice President
Engagement & Regulatory Affairs
AGC of California
Cell: 603-770-9264



Letter of Support - Port of LB.pdf
351K



PRESIDENT

Dina Kimble
Royal Electric Company

PRESIDENT-ELECT

Steve Rule
Turner Construction Company

VICE PRESIDENT

BUILDING
Matt Seals
Seals Construction

VICE PRESIDENT

HIGHWAY & TRANSPORTATION
Ural Yal
Flatiron West, Inc.

VICE PRESIDENT

UTILITY & INFRASTRUCTURE
Jim Blois
Blois Construction, Inc.

VICE PRESIDENT

SPECIALTY CONTRACTORS
Greg Timmerman
ISEC, Inc.

TREASURER

Pat Kelly
Granite Construction Company

IMMEDIATE PAST PRESIDENT

Mike Blach
Blach Construction Company

CEO

Peter Tateishi
AGC of California

HEADQUARTERS OFFICE

3095 Beacon Blvd.
West Sacramento, CA 95691
Office: 916.371.2422
Fax: 916.371.2352
member_services@agc-ca.org

Connie Chauv, City Planner
City of Los Angeles Planning Department
City Hall
200 North Spring Street, Room 720
Los Angeles, CA 90012

April 7, 2023

RE: LETTER OF SUPPORT FOR THE PORT OF LONG BEACH 9TH STREET HEAVY HAUL IMPROVEMENT ROUTE (2020-7285 GPA)

Dear Ms. Chauv:

We, at the Associated General Contractors of California (AGC of California), support the Port of Long Beach's (POLB's) 9th Street Heavy Haul Improvement Route. We advocate the use of the latest standards and best practices related to Building, Highway and Transportation, as well as Utility and Infrastructure. AGC of California values putting people first in everything we do. We do this in all our operations which includes focusing on safety, engaging the public and being mindful of the environment.

We support POLB's 9th Street Heavy Haul Improvement Route because it is crucial to our members and our employees' economic well-being. It is vital that this Project be completed on time for the economic well-being of our members and for the economic well-being of Southern California. If this Project is not completed in time (or at all), we will not be able to bring in the equipment that is needed for critical heavy civil construction projects. Our contractor members perform construction work on public works projects which include but are not limited to solar, wind, dams and reservoirs, road and highway improvements, bridges, landfills, greenery, and fire debris removal. Our members also perform heavy haul services so that we can get the equipment needed to perform this work.

If POLB's 9th Street Heavy Haul Project gets stalled, then we will not be able to get the equipment that is needed for projects. Then our members and our employees will be placed in economic jeopardy. This will also negatively impact the entire construction industry that performs the types of civil engineering projects mentioned above. The results will be layoffs in our industry as well as other sectors of the economy that rely on the construction industry. Another significant consequence is that if the 9th



Street Heavy Haul Improvement Route is not completed on time, private and public works projects will be significantly delayed or not be completed due to the inability to get the heavy equipment necessary for these projects.

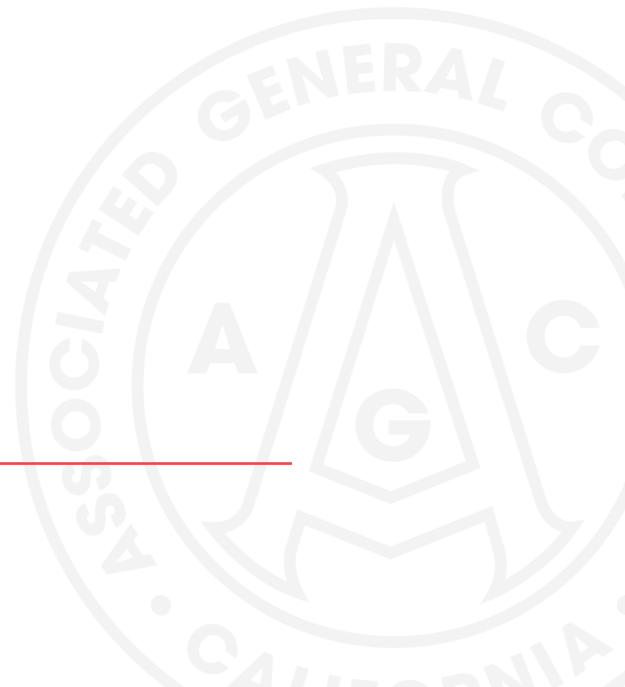
We urge you to work with the POLB and us to complete the 9th Street Heavy Haul Improvement Route on time. It is vitally needed for our members and our industry. Thank you.

Respectfully,

A handwritten signature in black ink that reads "Brian Mello".

Brian Mello
Associate Vice President of Engagement & Regulatory Affairs
Associated General Contractors of California

cc: Councilmember Tim McOsker, 15th Council District, City of Los Angeles
Matthew Lyman, POLB Intermodal Operations Coordinator





Connie Chauv <connie.chauv@lacity.org>

ROUTE (2020-7285 GPA)

Tuttle, Brad <brad.tuttle@heavy-trans.com>

Tue, Apr 11, 2023 at 2:16 PM

To: "connie.chauv@lacity.org" <connie.chauv@lacity.org>

Cc: "matthew.lyman@polb.com" <matthew.lyman@polb.com>, "strategicventuresbuild@gmail.com" <strategicventuresbuild@gmail.com>, "Peterson, Eric D." <eric.d.peterson@heavy-trans.com>

Dear Ms. Chauv,

Please accept this letter of support for the Port of Long Beach 9th St. heavy haul improvement project. If you have any questions, please do not hesitate to reach out.

Thank you,

Brad Tuttle

Anyone who can walk to the
welfare office, can walk to work

Sales, Projects, Equipment



office: 562 984 2455 | mobile: 562 481 6784

fax: 562 984 2469 |

email: brad.tuttle@heavy-trans.com

web: www.braggcrane.com

Integrity, Safety, Quality, Superior Service

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<http://www.braggcrane.com>

******Emails are difficult to compose. When reading my emails please do not assume an intended tone, voice, or inflection.******



041123 POLB 9th STREET CLOSURE.docx
245K



6251 Paramount Blvd., Long Beach, CA 90805
Phone (562) 984-2400 Fax (562) 984-2469

April 11, 2023

Connie Chauv, City Planner
City of Los Angeles Planning Department
City Hall
200 North Spring Street, Room 720
Los Angeles, CA 90012

**SUBJECT: LETTER OF SUPPORT FOR THE PORT OF LONG BEACH 9TH STREET HEAVY HAUL
IMPROVEMENT ROUTE (2020-7285 GPA)**

Dear Ms. Chauv:

We, at the Bragg Companies, which include Heavy Transport, support the Port of Long Beach's (POLB's) 9th Street Heavy Haul Improvement Route. Bragg Companies started in 1946 with only one crane and have now grown to be recognized as one of the largest integrated service crane and transportation companies in the construction industry.

Bragg Companies' core values are:

INTEGRITY

We consistently do what we say we are going to do with the utmost ethical behavior.

SAFETY

We always value the safety and health of our employees, customers, and work sites before profit or expediency.

QUALITY

We take pride in our promise of service with the highest standards of performance and results.

SUPERIOR SERVICE

We have the experience, talent, and capability to deliver superior service with unsurpassed value.

Due to our tenets, we would not support a project that jeopardized the well-being of our employees, the public, or the environment. Our equipment used to transport "permit loads" meets or surpasses all standards set by the California Air Resources Board.

We support POLB's 9th Street Heavy Haul Improvement Route because it is crucial to Bragg Companies' economic well-being. Most people do not realize how important the timely completion of this Project is to the economic well-being of Southern California. If this Project is not completed in time (or at all), we will not be able to bring in the equipment that is needed for critical heavy civil construction projects such as solar, wind, dams and reservoirs, road and highway improvements, bridges, landfills, greenery, and renewable fuels projects.





6251 Paramount Blvd., Long Beach, CA 90805
Phone (562) 984-2400 Fax (562) 984-2469

Again, if we cannot transport the equipment that is needed for heavy civil construction engineering, we will be placed in economic jeopardy. This will also negatively impact the entire heavy civil construction industry that performs the types of civil engineering projects mentioned above. The results will be layoffs in our industry as well as other sectors of the economy that rely on the construction industry. Additionally, it will have negative consequences for companies at the POLB that handle heavy construction equipment. Another consequence is that if the 9th Street Heavy Haul Improvement Route is not completed on time, private and public works projects will be significantly delayed or not be completed due to the inability to get the heavy equipment needed to perform this work.

We urge you to work with the POLB and Bragg Companies to complete the vitally needed 9th Street Heavy Haul Improvement Route on time. Thank you.

Sincerely,

A handwritten signature in black ink, appearing to read "BT" followed by a stylized flourish.

Brad Tuttle
Projects, Sales, Equipment

cc: Councilmember Tim McOsker, 15th Council District, City of Los Angeles
Matthew Lyman, POLB Intermodal Operations Coordinator
John Gasparo, President, Southern California Contractors Association





Monday, April 3, 2023

Connie Chauv, City Planner
200 North Spring Street, Room 720
Los Angeles, CA 90012

Re: CPC-2020-728- GPA

Dear Ms. Chauv:

On behalf of the oversize cargo customers in the Port of Long Beach, we wish to express our support for the subject General Plan Amendment application submitted by the Port of Long Beach as part of the Pier B On-Dock Rail Support Facility Program (Program).

The 9th Street and Pico Avenue oversized truck route is currently the only route of its kind in the region and is scheduled to be closed next year to allow for the start of construction of the Pier B On-Dock rail project.

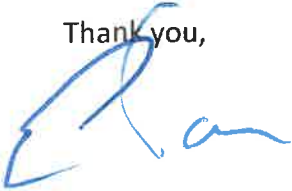
Unless the proposed Anaheim Way Heavy Haul route is constructed, there will be no capability to import or export oversized cargo. The closest alternative gateway with the capability to handle this type of cargo is the Gulf port of Houston, Texas. Without the proposed Anaheim Way Heavy haul Route the motor carriers currently serving this market segment will need to re-allocate, divest, or re-purpose their specialized assets, and either find new market segments to serve, or relocate their businesses to other, more capable gateways.

The value of maintaining our capability to handle oversized cargo includes the following local and regional benefits:

1. An oversized cargo route is required to move the huge transformers, compressors, and vaporizers that are going into the region's badly needed electrical infrastructure projects. If there is no oversized route available in the Port of Long Beach, these massive pieces of cargo will need to come in over Houston and move into the region by truck from Texas, which will add significant cost and complexity to these critical infrastructure projects.
2. The tenant terminal operator at our Pier F facility relies on oversized cargo as a significant portion of their business. If our tenant is unable to serve the oversized cargo market, the business model for that facility may become untenable going forward, and they may be forced to close or relocate eliminating a significant number of ILWU work opportunities and management positions.

3. The Port of Long Beach is a strategic seaport for potential military movements. If the 9th Street crossing is permanently removed, and alternate route is not developed, the military's readiness capabilities may be significantly reduced on the West Coast.
4. The trailing equipment and other capital assets deployed by motor carriers to serve this market are highly specialized and extremely expensive. If the cargo moves over another gateway due to lack of capability in Long Beach, the highly specialized motor carrier community that serves that market will be completely displaced.
5. The ability to move the giant steel pieces that are the components of major local projects by truck continues to be critical in reducing costs, complexity, and schedule timelines. This type of cargo generally moves on trailers that are between 120' and 220' in length, and without this project those trailers would have to transit a route that is bridge limited at 15'6", versus an actual average cargo height of 18'-22'.

Thank you,



Eric Sauer
Chief Executive Officer
California Trucking Association



Connie Chauv <connie.chauv@lacity.org>

LETTER OF SUPPORT - 9TH STREET HEAVY HAUL IMPROVEMENT ROUTE

1 message

Suzanne Scheideker Cook <strategicventuresbuild@gmail.com>

Fri, Apr 14, 2023 at 12:55 PM

To: "connie.chauv@lacity.org" <connie.chauv@lacity.org>

Cc: "Lyman, Matthew" <matthew.lyman@polb.com>, Rob Fleer <rfleer@contractorscargo.com>, Gerald Wheeler <gwheeler@contractorscargo.com>

Dear Ms. Chauv,

Please find the attached letter of support from Mr. Wheeler of Contractors Cargo. The importance of this Project is vital to the continued economic well-being of many diverse industries and their employees in Southern California.

Yours,
Suzanne Scheideker Cook
Strategic Ventures, Certified SBE

--

WWW.STRATEGICVENTURES.BUILD



04142023_Final_GWheeler_Contractors Cargo.pdf
232K

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10422 VRANA • HOUSTON, TX 77049 • (281) 456-9000 • FAX (281) 456-7366

April 14, 2023

Connie Chauv, City Planner
City of Los Angeles Planning Department
City Hall
200 North Spring Street, Room 720
Los Angeles, CA 90012

Dear Ms. Chauv:

SUBJECT: LETTER OF SUPPORT FOR THE PORT OF LONG BEACH 9TH STREET HEAVY HAUL IMPROVEMENT
ROUTE (2020-7285 GPA)

We, at Contractors Cargo, support the Port of Long Beach's (POLB's) 9th Street Heavy Haul Improvement Route. Contractors Cargo provides heavy haul transport to the military, aerospace, construction, general freight, machinery, mobile cranes, and windmill/green energy projects. An example of what Contractors Cargo does is that we moved the Space Shuttle "Enterprise" to the New Orleans World Fair.

Contractors Cargo recognizes the importance of safeguarding the environment. Due to this, we are committed to continuously updating equipment to comply with today's stringent environmental regulations. Our company's values would not support a project that jeopardized the well-being of our employees, the public or the environment.

We support POLB's 9th Street Heavy Haul Improvement Route. Without this Project, we will not be able to compete with companies operating from other ports in Northern California and in other states. The result will threaten our company's existence. It will also threaten our region's ability to do solar, wind, dams and reservoirs, road and highway improvements, bridges, landfills, greenery, fire debris removal, military, and aerospace projects because Contractors Cargo will not be able to bring in/relocate the needed equipment. This, in turn, will negatively impact the economy in Southern California.

We urge you to work with the POLB and us to complete the 9th Street Heavy Haul Improvement Route on time. It is vitally needed. Thank you.

Respectfully,

Contractors Cargo Co.

A handwritten signature in cursive script that reads "Gerald D. Wheeler".

Gerald Wheeler

President

cc: Councilmember Tim McOsker, 15th Council District, City of Los Angeles
Matthew Lyman, POLB Intermodal Operations Coordinator



Connie Chauv <connie.chauv@lacity.org>

LETTER OF SUPPORT - PORT OF LONG BEACH 9TH STREET HEAVY HAUL IMPROVEMENT ROUTE

Suzanne Scheideker Cook <strategicventuresbuild@gmail.com>

Fri, Apr 14, 2023 at 1:30 PM

To: connie.chauv@lacity.org

Cc: Rob Fleeer <rfleeer@contractorscargo.com>, Gerald Wheeler <gwheeler@contractorscargo.com>, "Lyman, Matthew" <matthew.lyman@polb.com>, Diana Reyes Williams <strategicventuresbuild.diana@gmail.com>

Dear Ms. Chauv,

We are forwarding a letter of support from Mr. Rob Fleeer of Contractors Cargo for this incredibly important Project by the Port of Long Beach.

Yours,
Suzanne Scheideker Cook
Strategic Ventures, Certified SBE

--

WWW.STRATEGICVENTURES.BUILD



04142023_Final_RFleeer_Contractors Cargo_Letter of Support_POLB 9th Street Project.pdf

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• MC 129712 • DOT 92693

April 14, 2023

Connie Chauv, City Planner
City of Los Angeles Planning Department
City Hall
200 North Spring Street, Room 720
Los Angeles, CA 90012

Dear Ms. Chauv:

SUBJECT: LETTER OF SUPPORT FOR THE PORT OF LONG BEACH 9TH STREET HEAVY
HAUL IMPROVEMENT ROUTE (2020-7285 GPA)

We, at Contractors Cargo, support the Port of Long Beach's (POLB's) 9th Street Heavy Haul Improvement Route. Contractors Cargo provides heavy haul transport to the military, aerospace, construction, general freight, machinery, mobile cranes, and windmill/green energy projects. An example of what Contractors Cargo does is that we moved the Space Shuttle "Enterprise" to the New Orleans World Fair.

Contractors Cargo recognizes the importance of safeguarding the environment. Due to this, we are committed to continuously updating equipment to comply with today's stringent environmental regulations. Our company's values would not support a project that jeopardized the well-being of our employees, the public or the environment.

We support POLB's 9th Street Heavy Haul Improvement Route. Without this Project, we will not be able to compete with companies operating from other ports in Northern California and in other states. The result will threaten our company's existence. It will also threaten our region's ability to do solar, wind, dams and reservoirs, road and highway improvements, bridges, landfills, greenery, fire debris removal, military, and aerospace projects because Contractors Cargo will not be able to bring in/relocate the needed equipment. This, in turn, will negatively impact the economy in Southern California.

We urge you to work with the POLB and us to complete the 9th Street Heavy Haul Improvement Route on time. It is vitally needed. Thank you.

Respectfully,

Rob Fleer
Operations Manager
Contractors Cargo Company
(310) 609-1957

cc: Councilmember Tim McOsker, 15th Council District, City of Los Angeles
Matthew Lyman, POLB Intermodal Operations Coordinator



Connie Chauv <connie.chauv@lacity.org>

Heavy Haul Truckers - Letter of Support

Irene Huerta | Admin Asst to President <irene.huerta@ilwu13.org>

Fri, Apr 14, 2023 at 10:32 AM

To: "connie.chauv@lacity.org" <connie.chauv@lacity.org>

Cc: Gary Herrera | President <gary.herrera@ilwu13.org>, Sal DiCostanzo <sal.dicostanzo@ilwu13.org>

Good morning, attached is ILWU Local 13's letter of support.

Irene Huerta

Administrative Assistant – President's Office International Longshore Warehouse Union (ILWU) – Local 13

[630 S. Centre Street](#)

[San Pedro, CA 90731](#)

(310) 830-1130 x115 – Office

(310) 874-8384 Cell

(310) 830-0931 Fax

irene.huerta@ilwu13.org

www.ilwu13.com

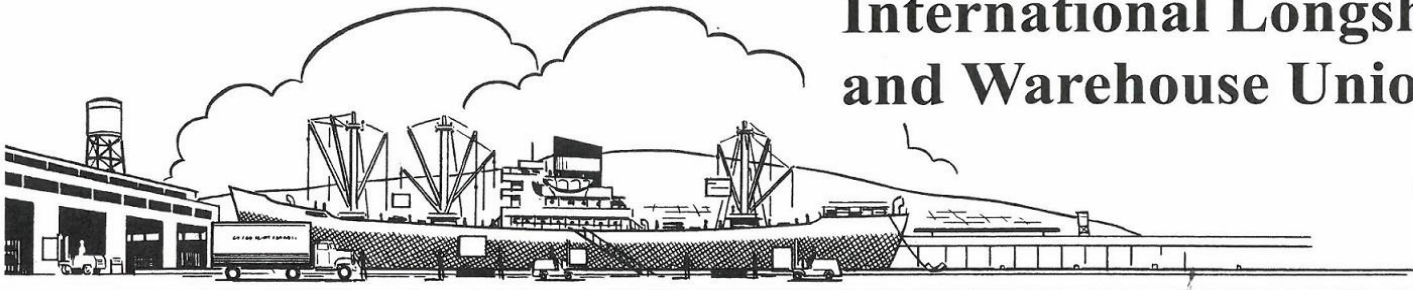
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04-14-23 Heavy Haul Truckers - Letter of Support.pdf
602K

International Longshore and Warehouse Union



ILWU Local 13 • 630 Centre Street • San Pedro, CA 90731 • (310) 830-1130

April 14, 2023

City of Los Angeles
Connie Chauv, City Planner
200 North Spring Street, Room 720
Los Angeles, CA 90012

Re: CPC-2020-728- GPA

Dear Ms. Chauv,

On behalf of the International Longshore and Warehouse Union, Local 13 (ILWU) we wish to express our support for the Subject General Plan Amendment Application submitted by the Port of Long Beach (Port) as part of the Pier B On-Dock Rail Support Facility Program (Program).

The 9th Street and Pico Avenue oversized truck route is currently the only route of its kind in the region and is scheduled to be closed next year to allow for the start of construction of the Pier B On-Dock rail project.

Unless the proposed Anaheim Way Heavy Haul route is constructed, there will be no capability to import or export oversized cargo. The closest alternative gateway with the capability to handle this type of cargo is the Gulf Port of Houston, Texas. Without the proposed Anaheim Way Heavy Haul Route, the motor carriers currently serving this market segment will need to re-allocate, divest, or re-purpose their specialized assets, and either find new market segments to serve, or relocate their businesses to other, more capable gateways.

The value of maintaining the Port's capability to handle oversized cargo includes the following local and regional benefits:

1. An oversized cargo route is required to move the huge transformers, compressors, and vaporizers that are going into the region's badly needed electrical infrastructure projects. If there is no oversized route available in the Port of Long Beach, these massive pieces of cargo will need to come into the United States through Houston and move into the region by truck from Texas, which will add significant cost and complexity to these critical infrastructure projects.
2. One of our local employers, the tenant terminal operator at the Pier F facility relies on oversized cargo as a significant portion of their business. If our employer is unable to serve

the oversized cargo market, the business model for that facility may become untenable going forward, and they may be forced to close or relocate eliminating a significant number of our work opportunities. Many of these workers are residents of the adjacent community of Wilmington, CA.

3. The Port of Long Beach is also a strategic seaport for potential military movements. If the 9th Street crossing is permanently removed, and an alternate route is not developed, the military's readiness capabilities may be significantly reduced on the West Coast.
4. The trailing equipment and other capital assets deployed by motor carriers to serve this market are highly specialized and extremely expensive. If the cargo moves over another gateway, due to a lack of capability in Long Beach, the highly specialized motor carrier community that serves this market will be completely displaced.
5. Lastly, the ability to move the giant steel pieces that are the components of major local projects, by truck, continues to be critical in reducing costs, complexity, and schedule timelines. This type of cargo generally moves on trailers that are between 120' and 220' in length, and without this project those trailers would have to transit a route that is bridge limited to 15'6", versus an actual average cargo height of 18'-22'.

Thank you for your consideration. Please feel free to contact us should you have any additional questions.

Sincerely,



Gary Herrera
President
ILWU Local 13



Connie Chauv <connie.chauv@lacity.org>

LETTER OF SUPPORT - PORT OF LONG BEACH 9TH STREET HEAVY HAUL IMPROVEMENT ROUTE

Suzanne Scheideker Cook <strategicventuresbuild@gmail.com>

Fri, Apr 14, 2023 at 1:15 PM

To: connie.chauv@lacity.org

Cc: "Lyman, Matthew" <matthew.lyman@polb.com>, tim mcvay <tim@marcotrucking.com>, Robert Allred <robert@marcotrucking.com>, Bob Reed <bob@marcotrucking.com>, Tony Armenta <Tony@marcotrucking.com>

Dear Ms. Chauv,

We are forwarding a letter of support from Marco Transport Inc. for the critically needed 9th Street Heavy Haul Improvement Route by the Port of Long Beach.

Yours,

Suzanne Scheideker Cook
Strategic Ventures, Certified SBE.

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WWW.STRATEGICVENTURES.BUILD



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24K

MARCO TRANSPORT INC.
1501 N. Susan Street
Santa Ana, CA 92703

April 13, 2023

Connie Chauv, City Planner
City of Los Angeles Planning Department
City Hall
200 North Spring Street, Room 720
Los Angeles, CA 90012

Dear Ms. Chauv:

SUBJECT: LETTER OF SUPPORT FOR THE PORT OF LONG BEACH 9TH STREET HEAVY HAUL
IMPROVEMENT ROUTE (2020-7285 GPA)

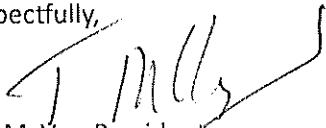
We, at Marco Transport, support the Port of Long Beach's (POLB's) 9th Street Heavy Haul Improvement Route. Marco Transport provides heavy haul transport to the military, aerospace, construction, general freight, machinery, mobile cranes, and windmill/green energy projects. Our employees are like our family. Due to this, we pay our employees good wages and provide good benefits. We work and reside in Southern California.

Marco Transport also recognizes the importance of safeguarding the environment. Due to this, Marco Transport is committed to continuously updating equipment to comply with today's stringent environmental regulations. Our company's values would not support a project that jeopardized the well-being of our employees, the public or the environment.

We support POLB's 9th Street Heavy Haul Improvement Route. Without this Project, we will not be able to compete with companies operating from other ports in Northern California. The result will threaten our company's existence. Marco Transport will also not be able to bring equipment needed for solar, wind, dams and reservoirs, road and highway improvements, bridges, landfills, greenery, fire debris removal, military, and aerospace projects.

We urge you to work with the POLB and us to complete the 9th Street Heavy Haul Improvement Route on time. It is vitally needed. Thank you.

Respectfully,



Tim McVay, President
Marco Transport

cc: Councilmember Tim McOsler, 15th Council District, City of Los Angeles
Matthew Lyman, POLB Intermodal Operations Coordinator



Connie Chauv <connie.chauv@lacity.org>

POLB - Heavy Haul Corridor Support Letter

Jon Switalski <jon@rebuildsocal.org>

Fri, Apr 14, 2023 at 7:54 AM

To: connie.chauv@lacity.org

Cc: Suzanne Scheideker Cook <strategicventuresbuild@gmail.com>, Mathew.lyman@polb.com, Amy Peake <amy@rebuildsocal.org>

Please find our letter attached.

Thank you,

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Jon Switalski
Executive Director

Jon@rebuildsocal.org

2400 E Katella Avenue, Suite 570
Anaheim, CA 92806
Office: (562) 483-2044

Rebuild SoCal Partnership
www.rebuildsocal.org



POLB_Heavy Haul Corridor_Support Letter.pdf
261K



2400 E. Katella Avenue, Suite 650
Anaheim, CA 92806

Office: +1 (562) 483 2044

April 13, 2023

Connie Chauv, City Planner
City of Los Angeles Planning Department
City Hall
200 North Spring Street, Room 720
Los Angeles, CA 90012

Dear Ms. Chauv:

SUBJECT: LETTER OF SUPPORT FOR THE PORT OF LONG BEACH 9TH STREET HEAVY HAUL
IMPROVEMENT ROUTE (2020-7285 GPA)

We at the Rebuild SoCal Partnership (RSCP), support the Port of Long Beach's (POLB's) 9th Street Heavy Haul Improvement Route. The RSCP comprises 2,750 contractors throughout Southern California, representing more than 90,000 union workers. We are dedicated to working with elected officials and educating the public about the essential infrastructure funding needed for airports, bridges, ports, rail, road, and water projects. Additionally, RSCP advocates for clean water, safe bridges, and roads. These are all vital to Southern California's economy.

We support POLB's 9th Street Heavy Haul Improvement Route because it is crucial to our members and our region's economic well-being. It is vital that this Project be completed to continue bringing in the equipment needed to maintain and build new infrastructure.

If POLB's 9th Street Heavy Haul Project gets stalled, our members will not be able to get the needed equipment for essential projects. This will negatively impact the entire construction industry. The results will be layoffs in our industry and other sectors of the economy that rely on the construction industry. This will cause a downturn in our region's economy.

We urge you to work with the POLB and us to complete the 9th Street Heavy Haul Improvement Route on time. It is vitally needed. Thank you.

Respectfully,

Jon Switalski, Executive Officer

Rebuild SoCal Partnership

cc: Councilmember Tim McOsker, 15th Council District, City of Los Angeles

Matthew Lyman, POLB Intermodal Operations Coordinator



Southern California District Council of Laborers (SCDCL) | Southwest Mountain States Regional Council of Carpenters (SWMSRCC)
The Operating Engineers (IUOE Local 12) | The Building Industry Association of Southern California (BIASC) | Associated General Contractors of California – AGC California
The Associated General Contractors of America – AGC San Diego Chapter | Engineering Contractors' Association (ECA)
Southern California Contractors Association (SCCA) | United Contractors (UCON)



Connie Chauv <connie.chauv@lacity.org>

LETTER OF SUPPORT - PORT OF LONG BEACH HEAVY HAUL IMPROVEMENT ROUTE

Suzanne Scheideker Cook <strategicventuresbuild@gmail.com>

Fri, Apr 14, 2023 at 2:40 PM

To: connie.chauv@lacity.org

Cc: "Lyman, Matthew" <matthew.lyman@polb.com>, John Gasparo <jgasparo@securitypaving.com>, Diana Reyes Williams <strategicventuresbuild.diana@gmail.com>

Dear Ms. Chauv,

We are forwarding the letter of support from John Gasparo, President, Southern California Contractors Association, for the Port of Long Beach's 9th Street Heavy Haul Improvement Route. As stated in Mr. Gasparo's letter, this Project is vital to the economic well-being of Southern California's construction industry.

Yours,

Suzanne Scheideker Cook
Strategic Ventures, Certified SBE

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WWW.STRATEGICVENTURES.BUILD



John Gasparo *President, Security Paving Company Inc.*
Kurt Kroner *Vice President, Kroner Environmental Services*
Gus Madrigal *Treasurer, Alcorn Fence Company*
Paul Marshall *Secretary, DRS Contracting*
Jon Gauthier *Immediate Past President, Salsbury Engineering, Inc.*

April 11, 2023

Connie Chauv, City Planner
City of Los Angeles Planning Department
City Hall
200 North Spring Street, Room 720
Los Angeles, CA 90012

Dear Ms. Chauv:

**SUBJECT: LETTER OF SUPPORT FOR THE PORT OF LONG BEACH 9TH STREET HEAVY HAUL
IMPROVEMENT ROUTE (2020-7285 GPA)**

We, at the Southern California Contractors Association (SCCA), support the Port of Long Beach's (POLB's) 9th Street Heavy Haul Improvement Route. SCCA's contractor members work and reside in Southern California. Additionally, all our contractor members are signatory to the trade unions which includes Operating Engineers, Laborers, Cement Masons, Carpenters, Ironworkers, and the Teamsters. This translates into good paying jobs and benefits for our members' employees.

SCCA also recognizes the importance of safeguarding the environment. Due to this, SCCA supports the Construction Industry's Air Quality Coalition (CIAQC) and the Construction Industry Coalition on Water Quality (CICWQ). CIAQC and CICWQ work with subject matter experts to create evidence and science-based air and water quality protection policies, plans, and regulations impacting the construction industry. CIAQC and CICWQ work closely with regulatory agencies such as the California Air Resources Board (CARB) in these endeavors. It is noteworthy that all our members' equipment used to haul "permit loads" and perform construction work meet or exceed all standards set by CARB and other environmental regulatory agencies.

Another core value of SCCA is the safety of our members, our employees, and the public. Our Safety Committee focuses on safety and environment compliance issues. As a result, SCCA is recognized throughout the industry as having an outstanding safety program.

Due to these core values, we would not support a project that jeopardized the well-being of our members, the public, or the environment.

We support POLB's 9th Street Heavy Haul Improvement Route because it is crucial to our members and our employees' economic well-being. It is vital that this Project be completed on time for the economic well-being of our members and for the economic well-being of Southern California. If this Project is not completed in time (or at all), we will not be able to bring in the

equipment that is needed for critical heavy civil construction projects. Our contractor members perform construction work on public works projects which include but are not limited to solar, wind, dams and reservoirs, road and highway improvements, bridges, landfills, greenery, and fire debris removal. Our members also perform heavy haul services so that we can get the equipment needed to perform this work.

If POLB's 9th Street Heavy Haul Project gets stalled, then we will not be able to get the equipment that is needed for projects. Then our members and our employees will be placed in economic jeopardy. This will also negatively impact the entire construction industry that performs the types of civil engineering projects mentioned above. The results will be layoffs in our industry as well as other sectors of the economy that rely on the construction industry. Another significant consequence is that if the 9th Street Heavy Haul Improvement Route is not completed on time, private and public works projects will be significantly delayed or not be completed due to the inability to get the heavy equipment necessary for these projects.

We urge you to work with the POLB and us to complete the 9th Street Heavy Haul Improvement Route on time. It is vitally needed for our members and our industry. Thank you.

Sincerely,



John Gasparo, President
Southern California Contractors Association

cc: Councilmember Tim McOsker, 15th Council District, City of Los Angeles
Matthew Lyman, POLB Intermodal Operations Coordinator